

01.03 - HIGHWAY DETAILS & MISC. INFO

INDEX OF DRAWINGS

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THE DESIGN APPEARS TO CONFORM TO APPLICABLE CRITERIA. APPROVAL IS NOT TO BE CONSTRUED TO MEAN THAT ALL ASPECTS OF THE DESIGN HAVE BEEN PERSONALLY CHECKED BY THE UNDERSIGNED.

TRANSPORTATION PRINCIPAL ENGINEER

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TRANSPORTATION PRINCIPAL ENGINEER

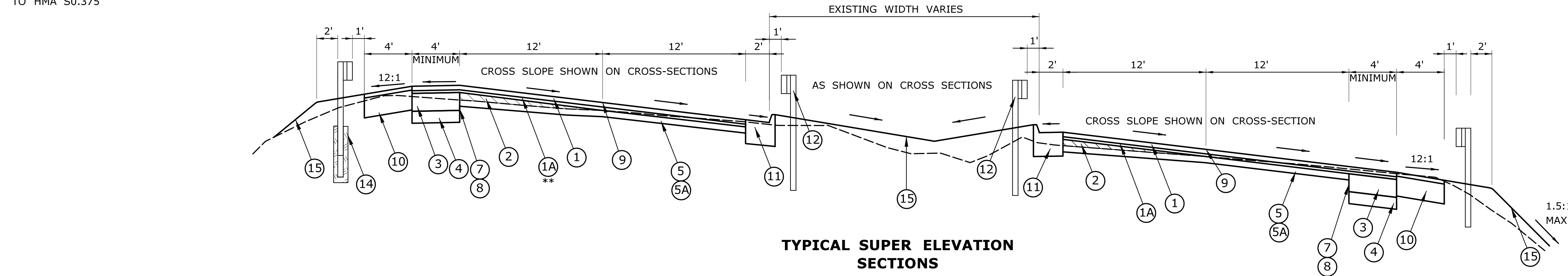
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GENERAL NOTES

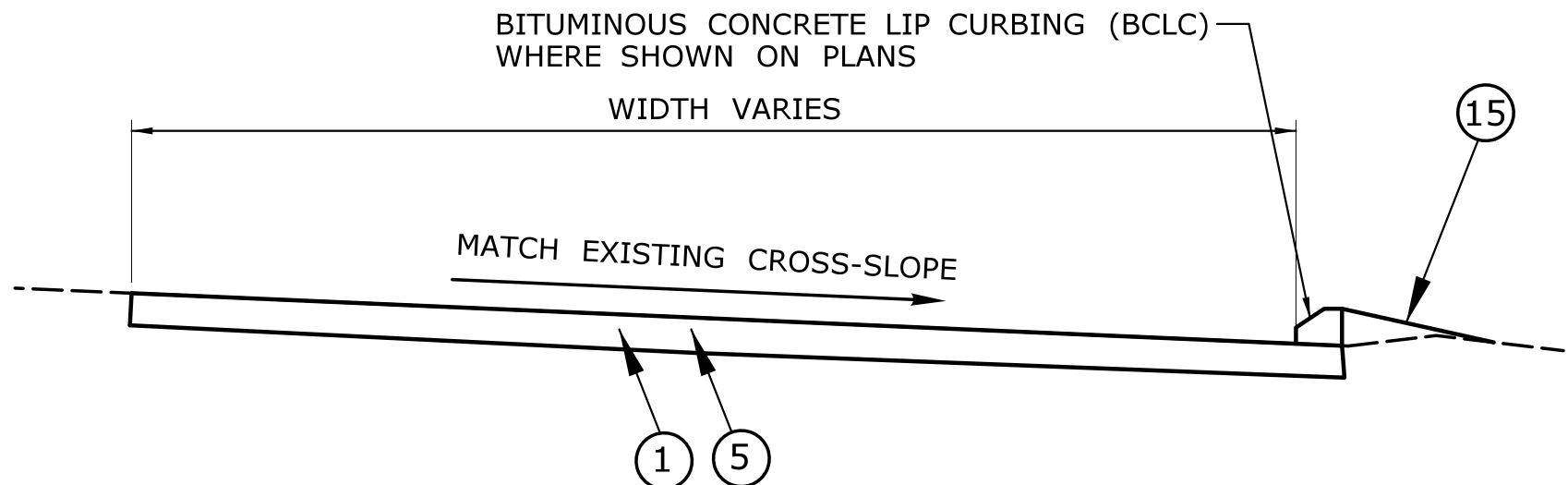
1. FOR PROPOSED SUPERELEVATION RATE SEE CROSS-SECTIONS.
2. SLOPES STEEPER THAN 2:1 REQUIRE EROSION CONTROL MATTING (SEE DETAIL).
3. SEE STAGE CONSTRUCTION PLANS FOR CROSS-SLOPE CORRECTION DETAILS.
*** THE HMA S0.25 FOR WEDGING SHALL BE PLACED WITH A THICKNESS OF 0.25" TO 1.5" PER LIFT PRIOR TO HMA S0.375

LEGEND

- ① 2" PMA S0.5 (FINAL WEARING SURFACE)
①A 1.5" HMA S0.375
② HMA S0.25 FOR WEDGING ***
③ 9" HMA S1 (PLACE ON 2 EQUAL LIFTS)
④ 6" SUBBASE
⑤ FINE MILLING OF BITUMINOUS CONCRETE
⑤A 2.25" HMA S0.5
⑥ MATERIAL FOR TACK COAT
⑦ CUT CONCRETE PAVEMENT
⑧ REMOVAL OF CONCRETE PAVEMENT
⑨ POINT OF APPLICATION OF GRADE
⑩ REINFORCED GRASS SHOULDER (SEE DETAILS)
⑪ MERRITT PARKWAY CONCRETE CURBING
⑫ MERRITT PARKWAY GUIDERAIL
⑬ MERRITT PARKWAY MEDIAN BARRIER
⑭ ROCK REMOVAL FOR GUIDE RAIL POSTS AS REQUIRED (SEE DETAIL)
⑮ 4" TOPSOIL WITH TURF ESTABLISHMENT
⑯ CUT BITUMINOUS CONCRETE PAVEMENT

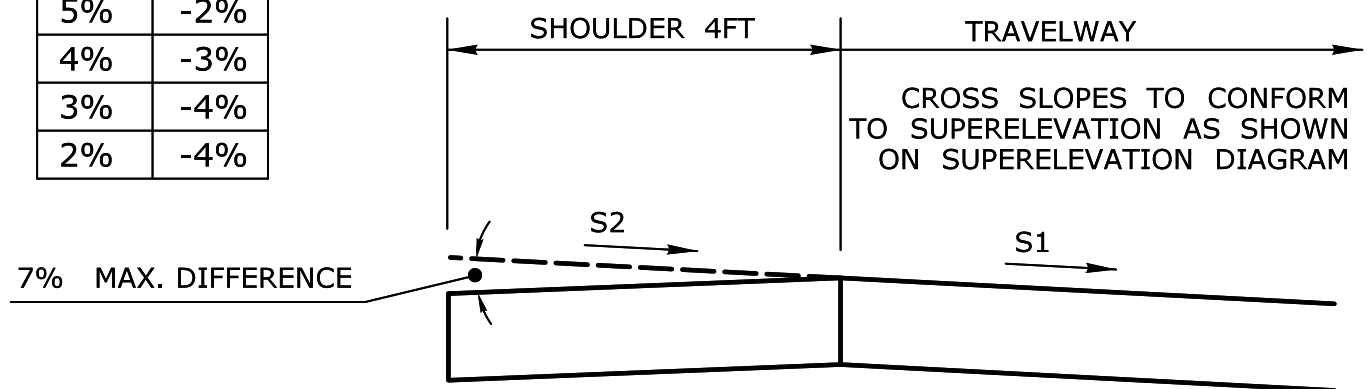


TYPICAL SUPER ELEVATION SECTIONS



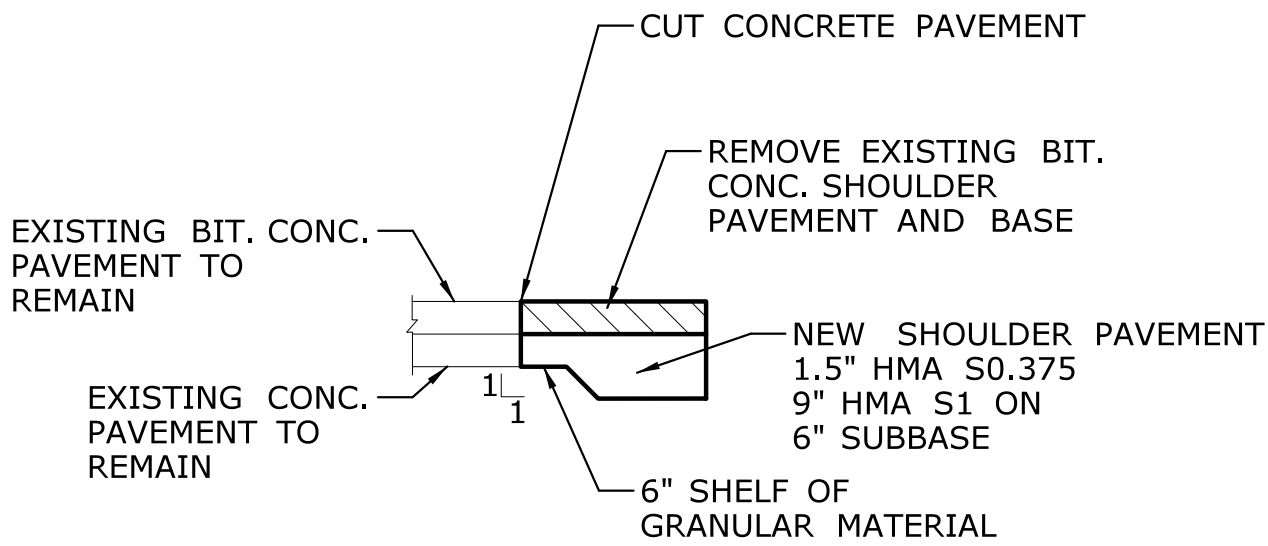
TYPICAL SECTION RAMP

S1	S2
8%	8%
7%	7%
6%	-1%
5%	-2%
4%	-3%
3%	-4%
2%	-4%

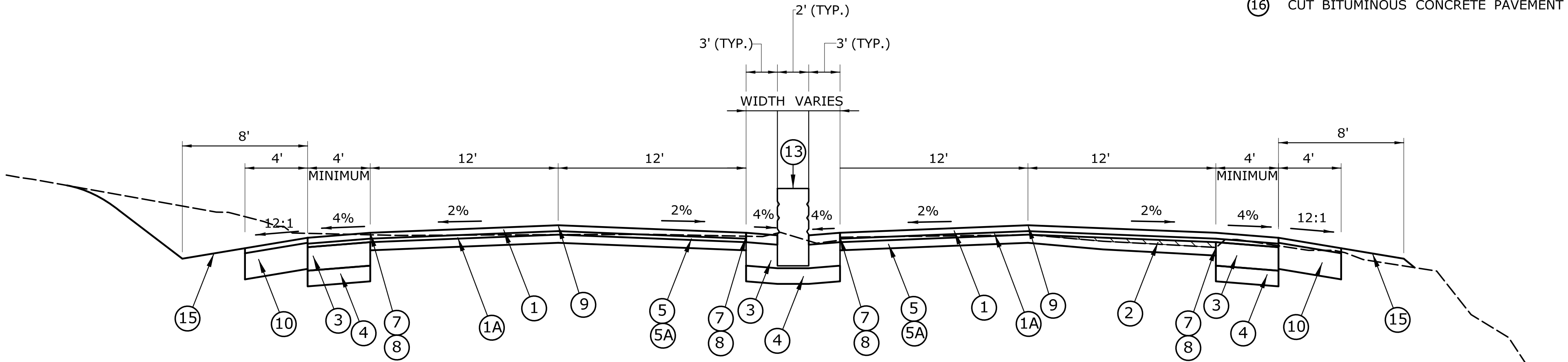


GENERAL NOTES:
1. MAY REQUIRE ATTACHABLE EXTENSIONS OR CUT-OFF PLATES ON PAVING EQUIPMENT.
2. WHERE S2 IS FLATTER THAN 2%, THE CONTRACTOR SHALL VERIFY THE CROSS SLOPE AT EACH 25FT INTERVAL (PAVEMENT TO BE INCLUDED IN THE COST OF THE BITUMINOUS CONCRETE PAY ITEM-THERE WILL BE NO DIRECT PAYMENT)

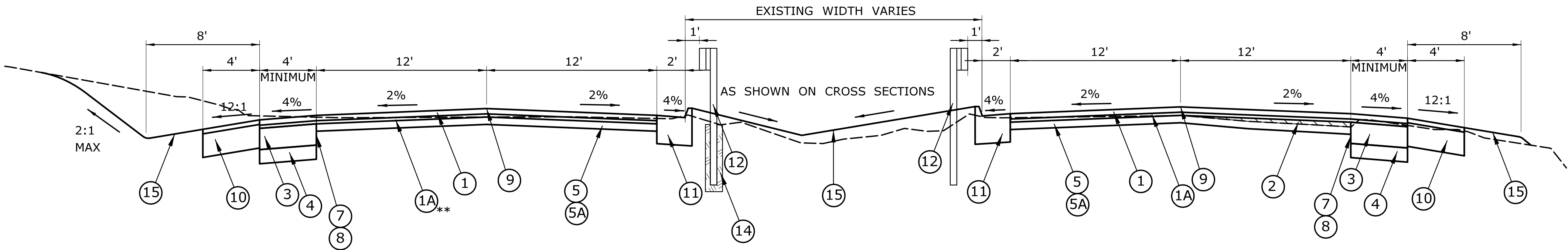
TYPICAL SHOULDER TREATMENT AT HIGH SIDE OF ROADWAY



FULL DEPTH CONSTRUCTION ADJACENT TO CONCRETE PAVEMENT



TYPICAL SECTION MERRITT PARKWAY MEDIAN BARRIER



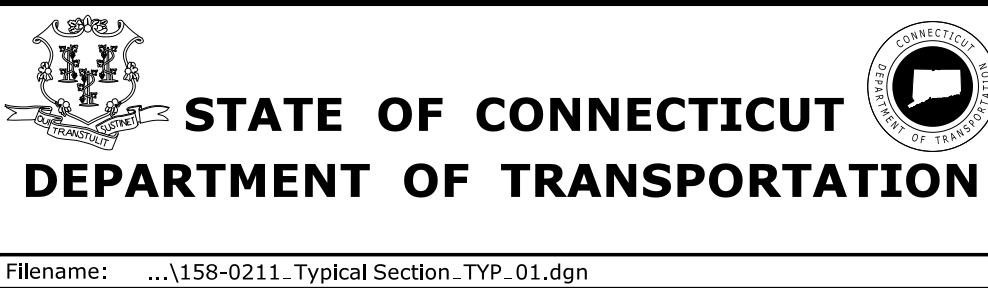
TYPICAL SECTION NORMAL CROWN

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
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Plotted Date: 7/18/2016

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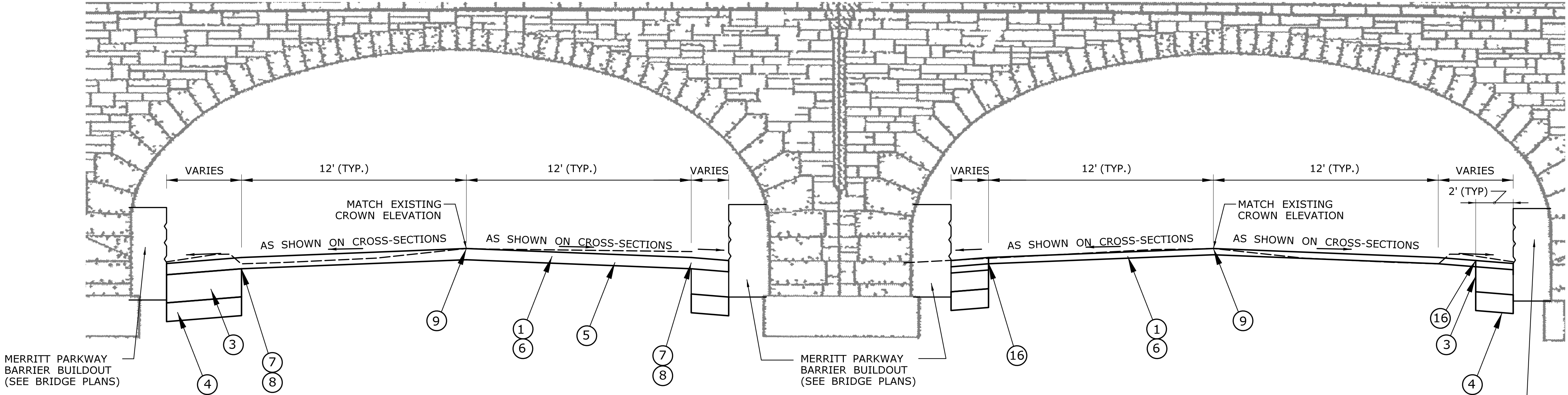
SIGNATURE/
BLOCK:
OFFICE OF ENGINEERING
APPROVED BY: *Willie B. Smith* DATE:

PROJECT TITLE:
**MERRITT PARKWAY (ROUTE 15)
SAFETY IMPROVEMENTS, RESURFACING,
ENHANCEMENTS AND BRIDGE IMPROVEMENTS**

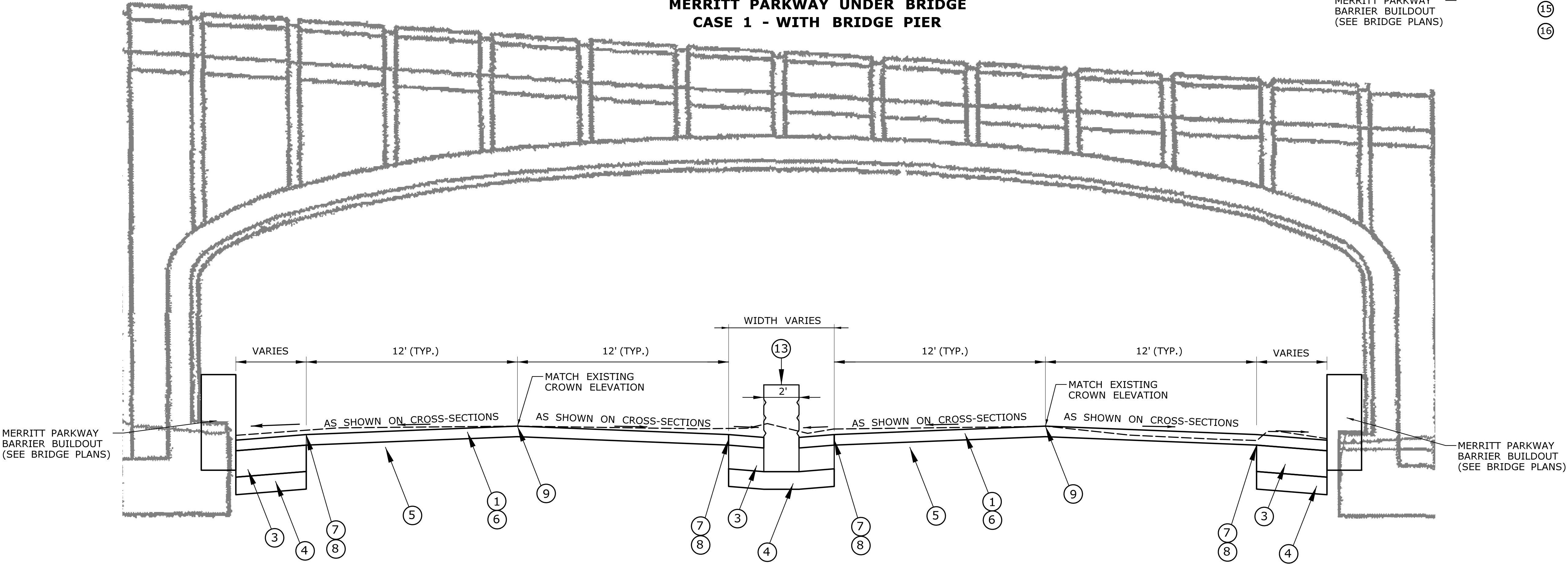
TOWN:
**WESTPORT
FAIRFIELD**
DRAWING TITLE:
TYPICAL SECTION
PROJECT NO.
158-0211
DRAWING NO.
TYP-01
SHEET NO.
01.03.02

GENERAL NOTES

1. FOR PROPOSED SUPERELEVATION RATE SEE CROSS-SECTIONS.
2. SLOPES STEEPER THAN 2:1 REQUIRE EROSION CONTROL MATTING (SEE DETAIL).
3. SEE STAGE CONSTRUCTION PLANS FOR CROSS-SLOPE CORRECTION DETAILS.
- *** THE HMA S0.25 FOR WEDGING SHALL BE PLACED WITH A THICKNESS OF 0.25" TO 1.5" PER LIFT PRIOR TO HMA S0.375



TYPICAL SECTION
MERRITT PARKWAY UNDER BRIDGE
CASE 1 - WITH BRIDGE PIER



TYPICAL SECTION
MERRITT PARKWAY UNDER BRIDGE
CASE 2 - WITHOUT BRIDGE PIER

LEGEND

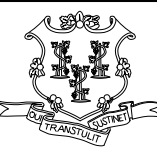
- 1 2" PMA S0.5 (FINAL WEARING SURFACE)
- 1A 1.5" HMA S0.375
- 2 HMA FOR WEDGING ***
- 3 9" HMA S1 (PLACE ON 2 EQUAL LIFTS)
- 4 6" SUBBASE
- 5 FINE MILLING OF BITUMINOUS CONCRETE
- 5A 2.25" HMA S0.5
- 6 MATERIAL FOR TACK COAT
- 7 CUT CONCRETE PAVEMENT
- 8 REMOVE CONCRETE PAVEMENT
- 9 POINT OF APPLICATION OF GRADE
- 10 REINFORCED GRASS SHOULDER (SEE DETAILS)
- 11 MERRITT PARKWAY CONCRETE CURBING
- 12 MERRITT PARKWAY GUIDERAIL
- 13 MERRITT PARKWAY MEDIAN BARRIER
- 14 ROCK REMOVAL FOR GUIDE RAIL POSTS/BARRIERS, AS REQUIRED
- 15 4" TOPSOIL WITH TURF ESTABLISHMENT
- 16 CUT BITUMINOUS CONCRETE PAVEMENT


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Plotted Date: 7/18/2016

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**STATE OF CONNECTICUT**
DEPARTMENT OF TRANSPORTATION



Filename: ...\\158-0211_Typical Section..TYP_02.dgn

SIGNATURE/
BLOCK:
OFFICE OF ENGINEERING

APPROVED BY: DATE:



PROJECT TITLE:
**ROUTE 15 SAFETY IMPROVEMENTS,
RESURFACING, ENHANCEMENTS,
AND BRIDGE IMPROVEMENTS**

TOWN:
**WESTPORT
FAIRFIELD**

DRAWING TITLE:
TYPICAL SECTION

PROJECT NO.
158-0211

DRAWING NO.
TYP-02

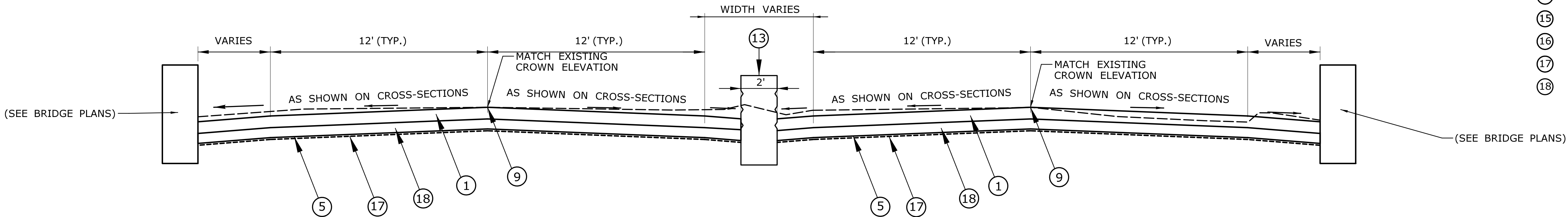
SHEET NO.
01.03.03

GENERAL NOTES

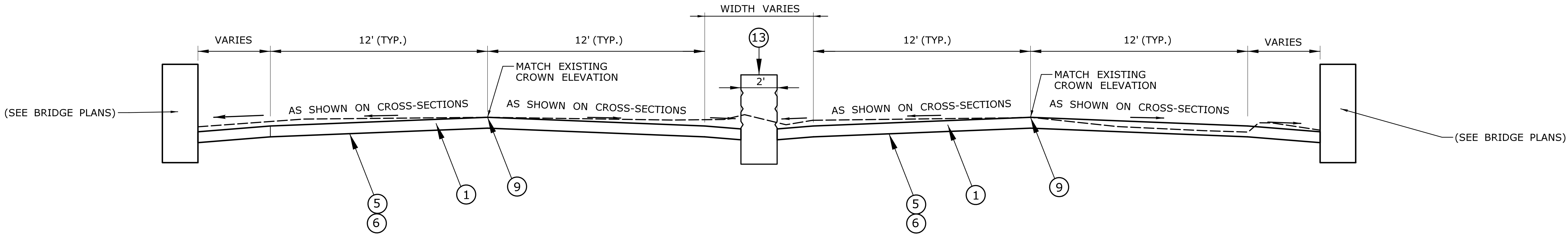
1. FOR PROPOSED SUPERELEVATION RATE SEE CROSS-SECTIONS.
2. SLOPES STEEPER THAN 2:1 REQUIRE EROSION CONTROL MATTING (SEE DETAIL).
3. SEE STAGE CONSTRUCTION PLANS FOR CROSS-SLOPE CORRECTION DETAILS.
4. THE DETAILS PROVIDED ON THIS SHEET PROVIDE MILLING AND PAVING INFORMATION FOR BRIDGES CARRYING THE MERRITT PARKWAY OVER A LOCAL ROAD OR WATERCOURSE. THE CONTRACTOR SHALL REGERENCE THE STRUCTURE SHEETS WHEN PERFORMING WORK ON BRIDGES TO DETERMINE THE MILLING OPERATION SCHEDULE IN CONJUNCTION WITH STRUCTURE STAGING.
5. BRIDGE NOS. 731 AND 5763 SHALL RECEIVE SPECIAL ATTENTION. THESE BRIDGES ARE TO BE MILLED, PATCHED, MEMBRANED, AND PAVED PER THE STRUCTURE SHEETS.
- ***THE HMA S0.25 FOR WEDGING SHALL BE PLACED WITH A THICKNESS OF 0.25" TO 1.5" PER LIFT PRIOR TO HMA S0.375

LEGEND



- 1 2" PMA S0.5 (FINAL WEARING SURFACE)
- 1A 1.5" HMA S0.375
- 2 HMA FOR WEDGING ***
- 3 9" HMA S1 (PLACE ON 2 EQUAL LIFTS)
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- 13 MERRITT PARKWAY MEDIAN BARRIER
- 14 ROCK REMOVAL FOR GUIDE RAIL POSTS/BARRIERS, AS REQUIRED
- 15 4" TOPSOIL WITH TURF ESTABLISHMENT
- 16 CUT BITUMINOUS CONCRETE PAVEMENT
- 17 WATERPROOFING MEMBRANE (SEE STRUCTURE PLANS)
- 18 1" HMA S0.25



TYPICAL SECTION
BRIDGE CARRYING MERRITT PARKWAY
OVER LOCAL ROAD OR WATERCOURSE
BRIDGE NOS. 731, 5763



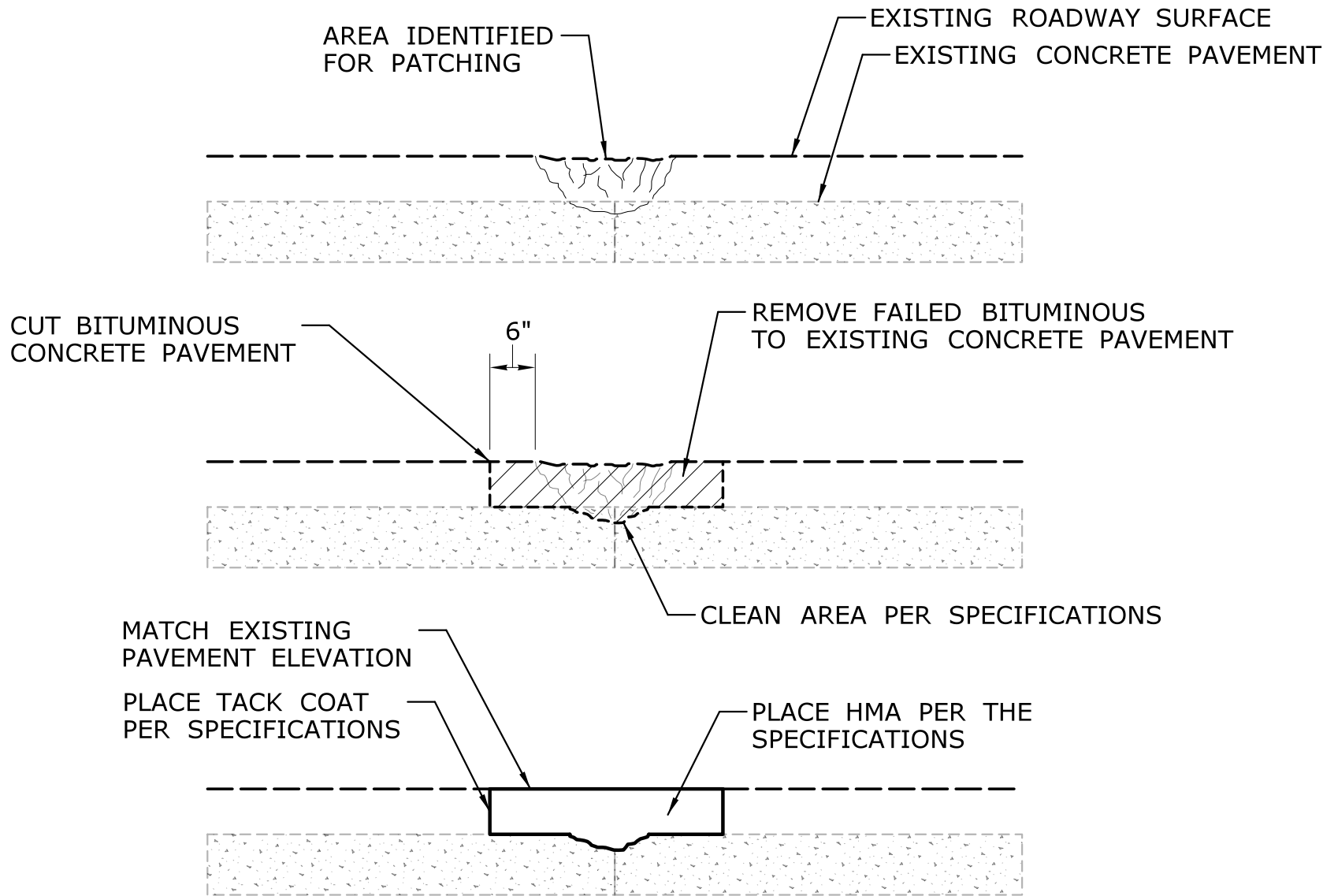
TYPICAL SECTION
BRIDGE CARRYING MERRITT PARKWAY
OVER LOCAL ROAD OR WATERCOURSE
BRIDGE NOS. 730, 733, 734

-	-	-	-	THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.	DESIGNER/DRAFTER:	 STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	SIGNATURE/ BLOCK: OFFICE OF ENGINEERING APPROVED BY: _____ DATE: _____ 	PROJECT TITLE: ROUTE 15 SAFETY IMPROVEMENTS, RESURFACING, ENHANCEMENTS, AND BRIDGE IMPROVEMENTS	TOWN:	WESTPORT FAIRFIELD	PROJECT NO.				
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STAGE 1 - GENERAL NOTES

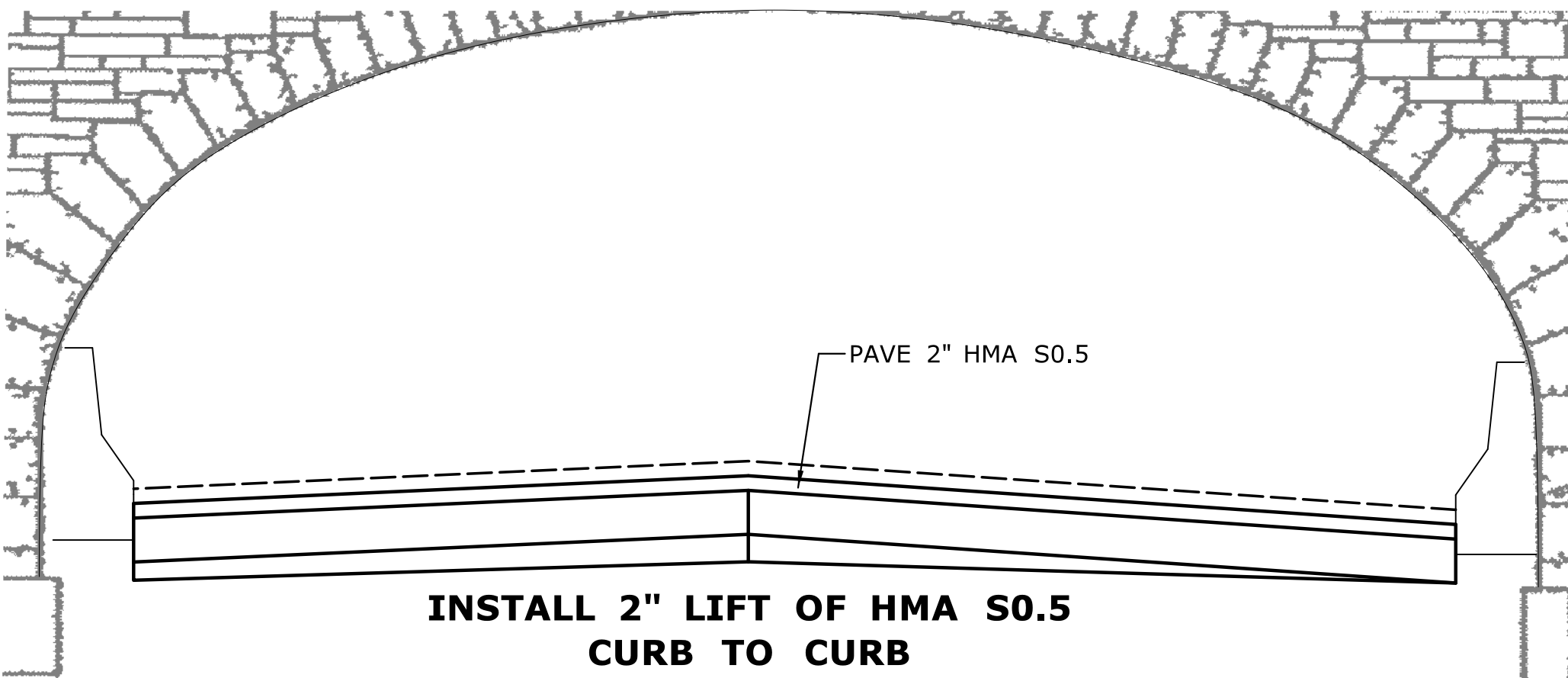
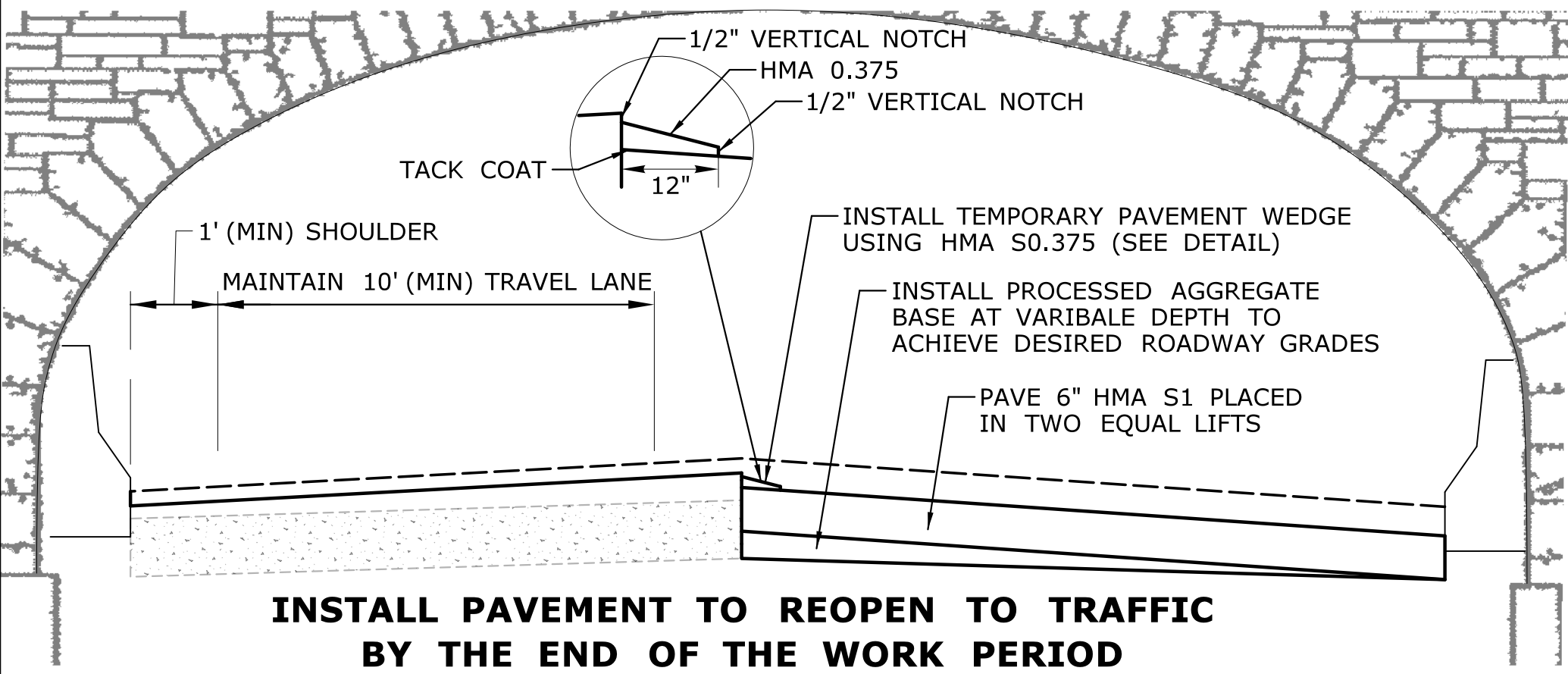
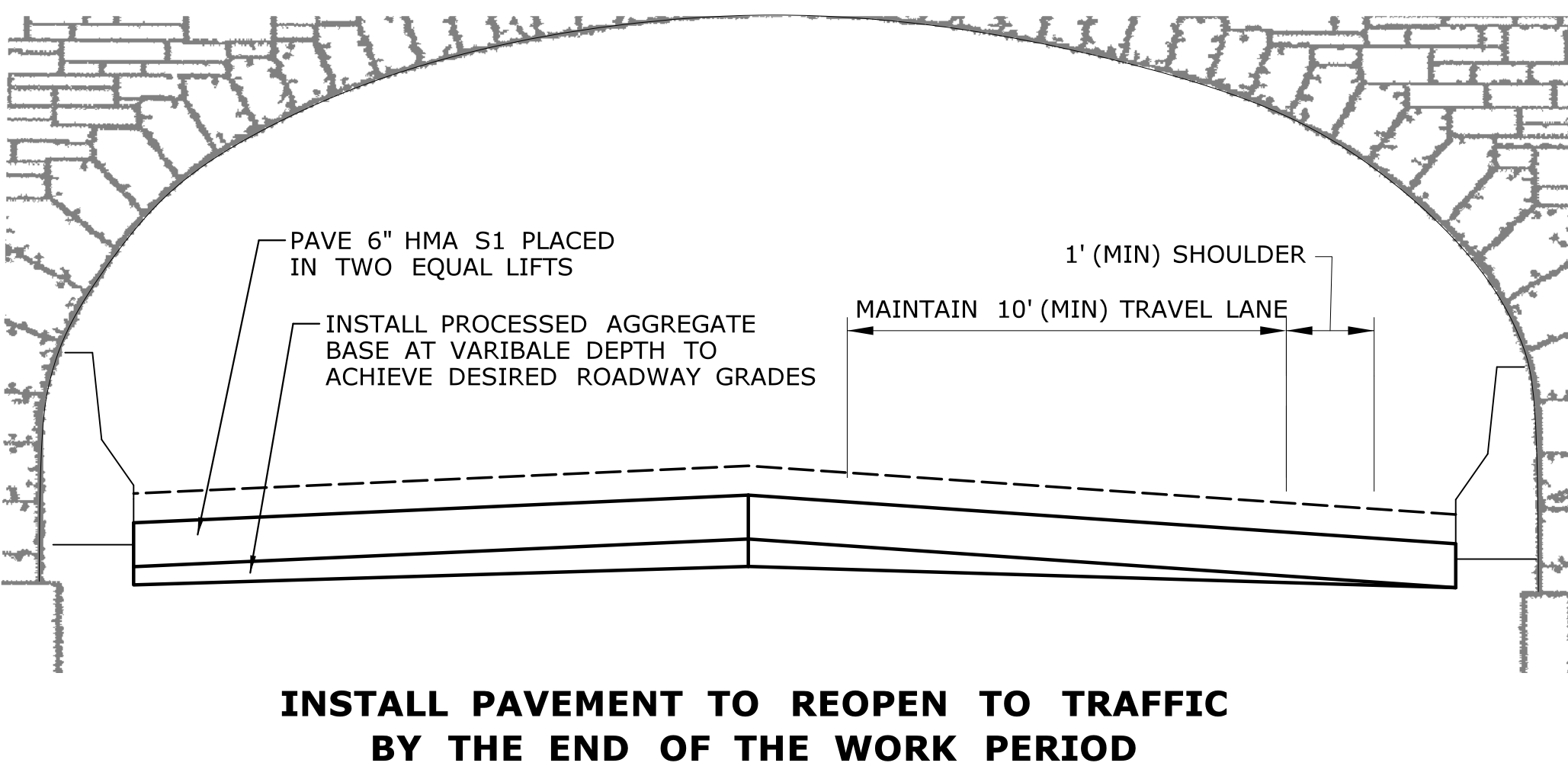
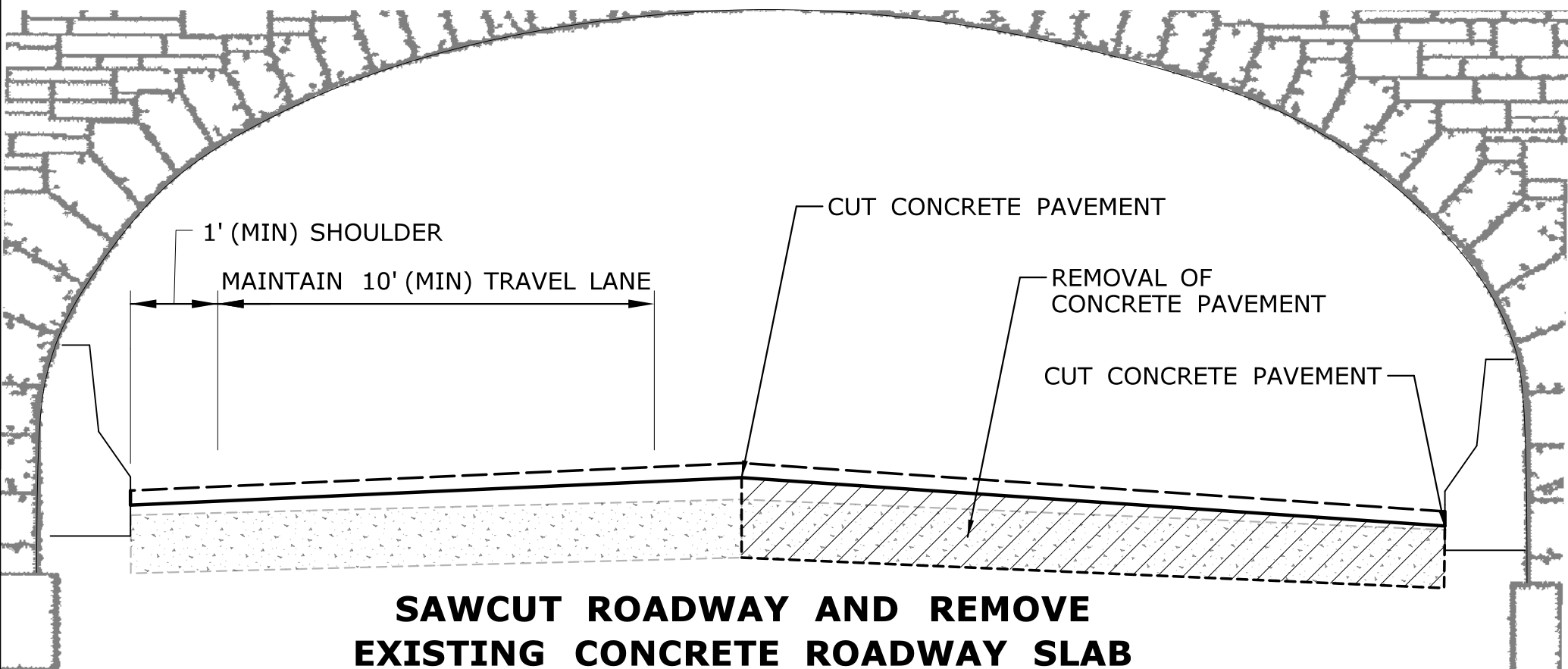
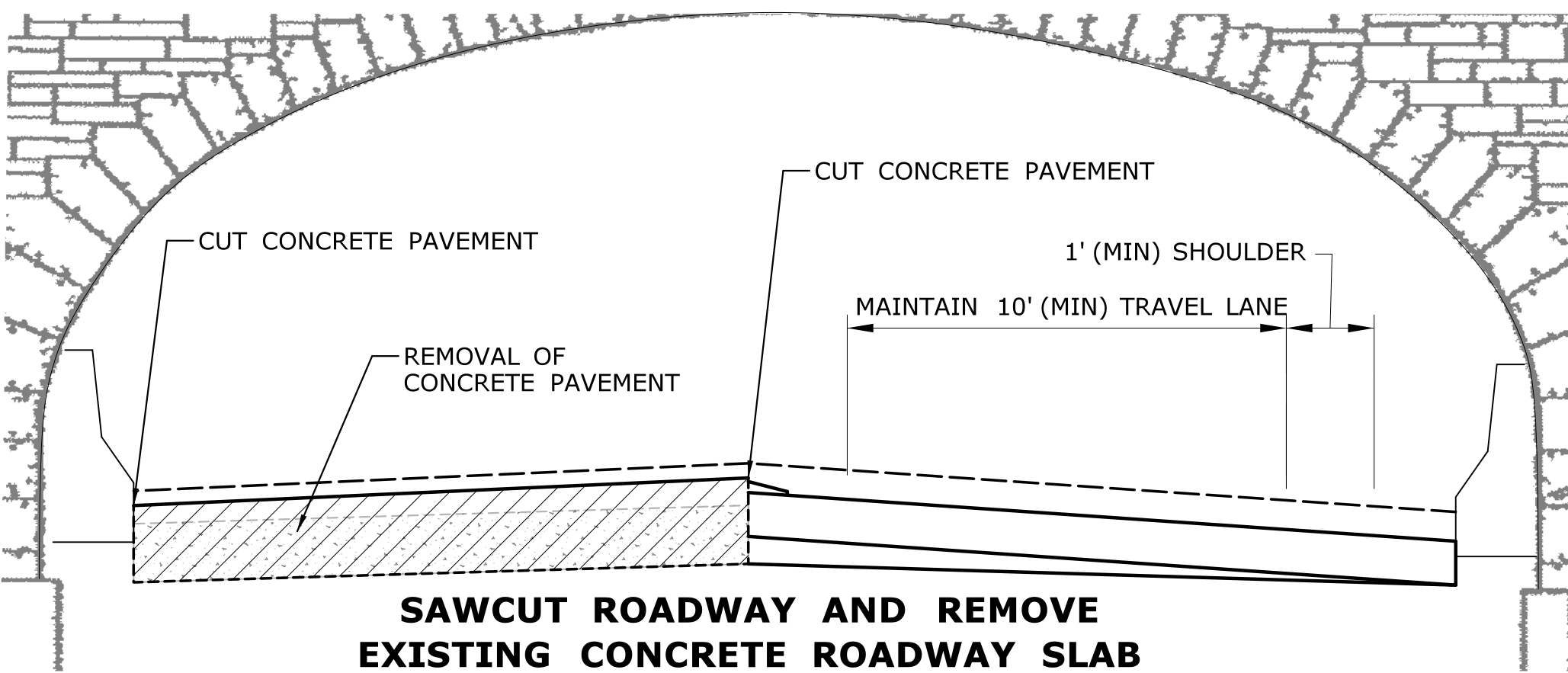
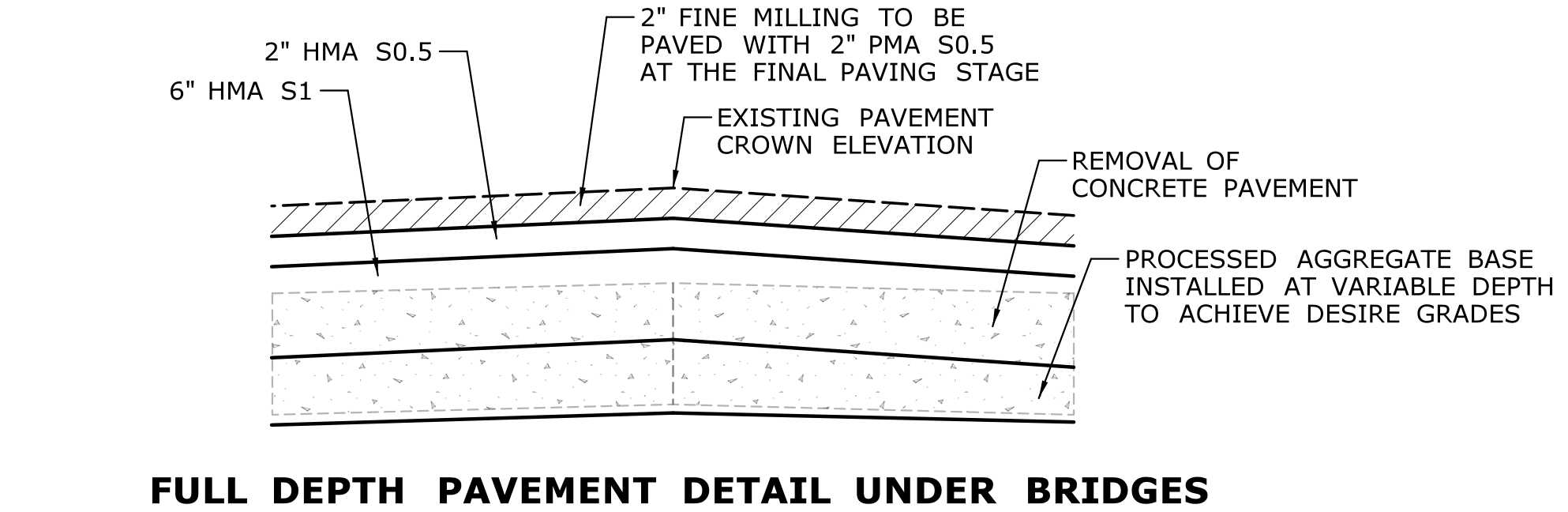
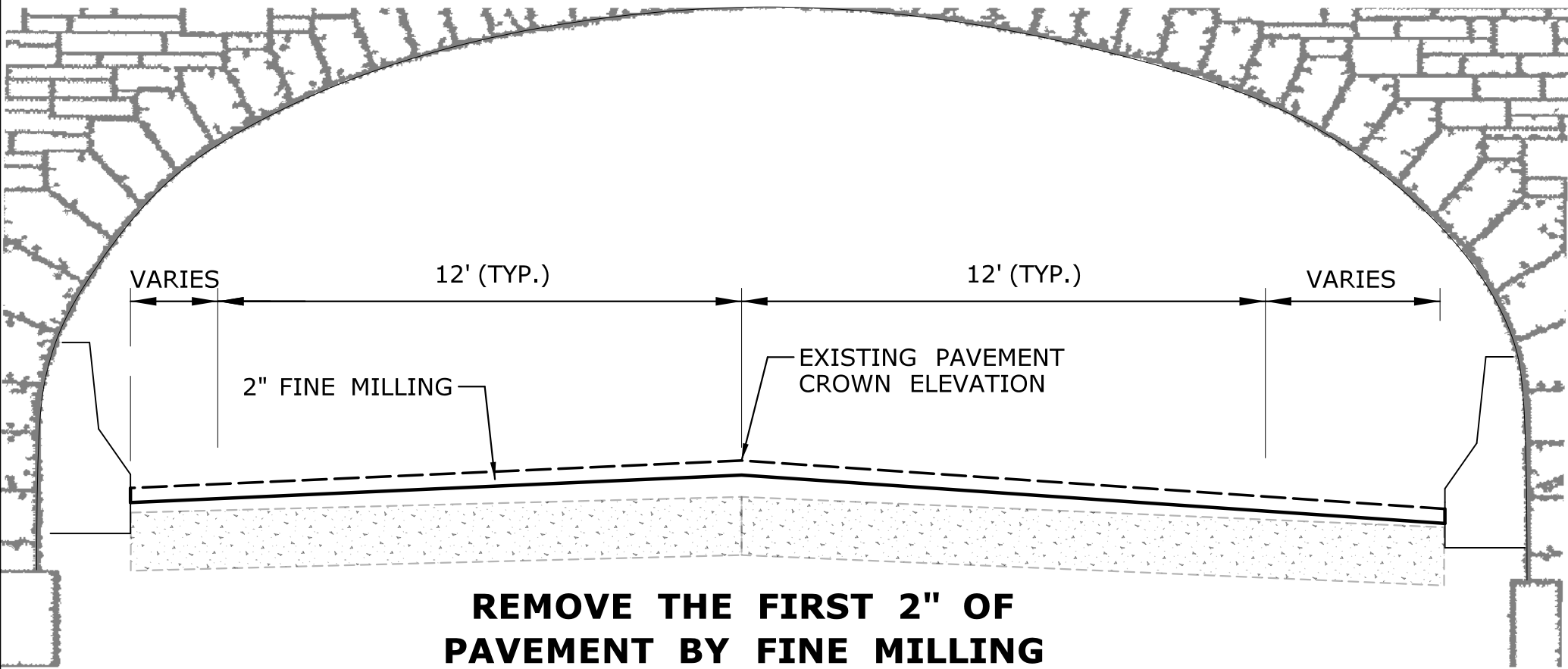
1. MAJOR WORK TO BE PERFORMED DURING STAGE 1 SHALL CONSIST OF "BITUMINOUS CONCRETE PATCHING - PARTIAL DEPTH", "REMOVAL OF CONCRETE PAVEMENT" UNDER BRIDGES, "CONSTRUCTION STAKING", AND "RESET CATCH BASIN" IN PREPARATION FOR STAGE 3 WORK. THIS WORK SHALL BE COMPLETED SIMULTANEOUSLY AND IN ADVANCE OF STAGE 2 OPERATIONS.
2. THE CONTRACTOR SHALL BE ALLOWED TO MAINTAIN TWO ACTIVE WORK AREAS CONCURRENTLY IN EACH DIRECTION. WORK AREAS SHALL NOT EXCEED A LENGTH OF 1.5 MILES AND SHALL BE A MINIMUM DISTANCE OF 1 MILE APART.
3. MINOR STAGE CONSTRUCTION WORK AREAS CAN BE APPROVED BY THE ENGINEER.
4. LANE AND SHOULDER CLOSURES SHALL BE IMPLEMENTED DURING ALLOWABLE PERIODS AS DICTATED IN THE SPECIAL PROVISION FOR SECTION 1.08.04 AND IN ACCORDANCE WITH THE SPECIAL PROVISION FOR ITEM NO. 0971001A.
5. THE CONTRACTOR SHALL BE ALLOWED TO UTILIZE A 'S-PATTERN' FOR MAINTENANCE AND PROTECTION OF TRAFFIC PURPOSES AS DESCRIBED IN THE SPECIAL PROVISION AND AS DIRECTED BY THE ENGINEER.
6. THE LIMITS OF "BITUMINOUS CONCRETE PATCHING - PARTIAL DEPTH" WILL BE DEFINED BY THE ENGINEER IN THE FIELD PRIOR TO THE START OF THE WORK.
7. THE LIMITS OF "REMOVAL OF CONCRETE PAVEMENT" UNDER BRIDGES DURING STAGE 1 ARE SHOWN ON THE HIGHWAY PLAN SHEETS.
8. THE CONTRACTOR SHALL PERFORM A SURVEY OF THE PAVED ROAD TO VERIFY GRADES AS DIRECTED UNDER SPECIAL PROVISION NO. 098001A - CONSTRUCTION STAKING
9. THE CONTRACTOR SHALL PERFORM A PRECONDITION SURVEY OF EXISTING DRAINAGE STRUCTURES IMPACTED BY THE SHIFTING OF TRAFFIC FOR STAGE 3. STRUCTURES THAT ARE DEEMED INCAPABLE OF CONVEYING TRAFFIC AS DETERMINED BY THE ENGINEER SHALL BE REPAIRED IN ACCORDANCE WITH SECTION 5.07 OF FORM 816. THE COST ASSOCIATED WITH THE PRECONDITION SURVEY SHALL BE INCLUDED IN THE GENERAL COST OF THE CONTRACT ITEM "MAINTENANCE AND PROTECTION OF TRAFFIC". THE COST ASSOCIATED WITH THE REPAIRS NECESSARY FOR THE SHIFTING OF TRAFFIC FOR STAGE CONSTRUCTION PURPOSES SHALL BE PAID AT THE CONTRACT UNIT PRICES FOR THOSE ITEMS REPAIRED UNDER SECTION 5.07 OF FOR 816. THE COST SHALL INCLUDE FURNISHING AND INSTALLING OF ALL MATERIALS, LABOR, EQUIPMENT, TRANSPORTATION, PROTECTION OF TRAFFIC, AND ANY OTHER NECESSARY INCIDENTALS THERETO, REQUIRED TO COMPLETE THESE REPAIRS.
10. TYPICAL M&PT SIGNING AND PAVEMENT MARKING PLANS WILL BE INCLUDED IN TRAFFIC ENGINEERING'S SPECIAL PROVISION FOR ITEM NO. 0971001A.
11. EXISTING CATCH BASINS, PIPES AND MANHOLES SHALL BE CLEANED AS DIRECTED BY THE ENGINEER THOSE CONTRACT ITEMS INCLUDES IN SECTION 6.53 OF FORM 816.

STAGE 1 - PARTIAL DEPTH PATCH
SUGGESTED SEQUENCE OF OPERATIONS



STAGE 1 - REMOVAL OF CONCRETE PAVEMENT UNDER BRIDGES
SUGGESTED SEQUENCE OF OPERATIONS

1. SAWCUT BITUMINOUS CONCRETE PAVEMENT AT LIMITS OF CONCETE PAVEMENT REMOVAL SHOWN ON PLAN SHEETS.
2. REMOVE THE FIRST 2" OF BITUMINOUS PAVEMENT THROUGH FINE MILLING.
3. INSTALL MAINTENANCE AND PROTECTION OF TRAFFIC AS REQUIRED.
4. CUT CONCRETE PAVEMENT AND REMOVE EXISTING CONCRETE ROADWAY SLAB.
5. INSTALL PROCESSED AGGREGATE BASE MATERIAL TO ACHIEVE DESIRED ROADWAY GRADES. ADDITIONAL MATERIAL MAY BE REQUIRED TO REPAIR DAMAGED SUBBASE AND SHALL BE INSTALLED AT THE DIRECTION OF THE ENGINEER.
6. PAVE 6" OF HMA S1 PLACED IN TWO EQUAL LIFTS.
7. INSTALL A TEMPORARY PAVEMENT WEDGE AS SHOWN ON THE DETAIL. TEMPORARY PAVEMENT WEDGE SHALL BE CONSTRUCTED OF HMA S0.375, INSTALLED HAND AND MECHANICALLY COMPACTED.
8. REOPEN THE ROADWAY TO TRAFFIC.
9. REPEAT STEPS 3 THROUGH 8 UNTIL THE ENTIRE AREA OF CONCRETE PAVEMENT REMOVAL AND FULL DEPTH RECONSTRUCTION HAVE BEEN COMPLETED.
10. PAVE 2" HMA S0.5, CURB TO CURB.
11. INSTALL TEMPORARY LINE STRIPING.

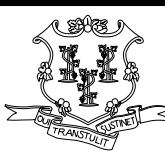



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Plotted Date: 7/18/2016

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**STATE OF CONNECTICUT**
DEPARTMENT OF TRANSPORTATION



Filename: ...\\158-0211-StagingPlan_STG_01.dgn

SIGNATURE/
BLOCK:
OFFICE OF ENGINEERING
APPROVED BY: *Willie B. Hall* DATE:

PROJECT TITLE:
**ROUTE 15 SAFETY IMPROVEMENTS,
RESURFACING, ENHANCEMENTS,
AND BRIDGE IMPROVEMENTS**

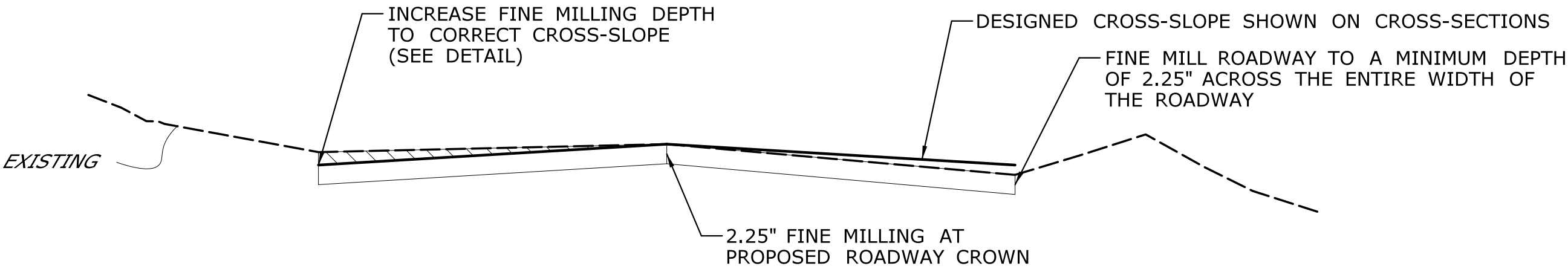
TOWN:
**WESTPORT
FAIRFIELD**
DRAWING TITLE:
**STAGE CONSTRUCTION
PLANS - STAGE 1**

PROJECT NO.
158-0211
DRAWING NO.
STG-01
SHEET NO.
01.03.05

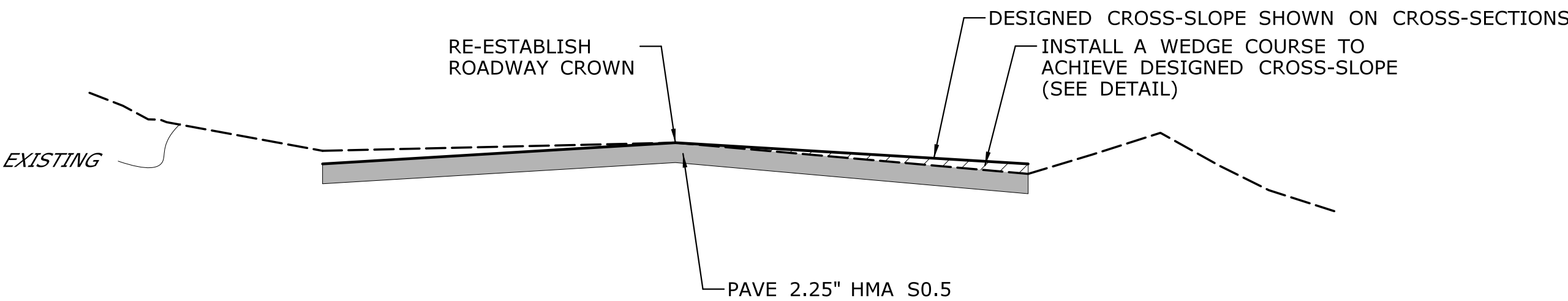
STAGE 2 - GENERAL NOTES

1. ALL WORK IN STAGES 1 & 2 SHALL BE COMPLETED PRIOR TO THE START OF WORK FOR STAGE 3.
2. THE CONTRACTOR SHALL BE ALLOWED TO MAINTAIN TWO ACTIVE WORK AREAS CONCURRENTLY IN EACH DIRECTION. WORK AREAS SHALL NOT EXCEED A LENGTH OF 1.5 MILES AND SHALL BE A MINIMUM DISTANCE OF 1 MILE APART.
3. MINOR STAGE CONSTRUCTION WORK AREAS CAN BE APPROVED BY THE ENGINEER.
4. LANE AND SHOULDER CLOSURES SHALL BE IMPLEMENTED DURING ALLOWABLE PERIOD AS DICTATED IN THE SPECIAL PROVISION FOR SECTION 1.08.04 AND IN ACCORDANCE WITH THE SPECIAL PROVISION FOR ITEM NO.0971001A.
5. THE CONTRACTOR SHALL BE ALLOWED TO UTILIZE A 'S-PATTERN' FOR MAINTENANCE AND PROTECTION OF TRAFFIC PURPOSES AS DESCRIBED IN THE SPECIAL PROVISION AND AS DIRECTED BY THE ENGINEER.
6. THE CONTRACTOR SHALL MILL A MINIMUM DEPTH OF 2.25" OF PAVEMENT TO REMOVE THE TOP COURSE OF EXISTING PAVEMENT AND CORRECT CROSS SLOPE AS INDICATED IN THE CROSS SECTIONS AND AS SHOWN IN THE DETAILS. ADDITIONAL PASSES OF FINE MILLING MAY BE REQUIRED TO ACHIEVE THE DESIGNED CROSS-SLOPE AS SHOWN ON DETAIL 'FINE MILLING TO CORRECT CROSS-SLOPE'.
7. CONCRETE CURBING MUST BE REMOVED IF IN CONFLICT WITH MILLING OPERATION.
8. IF CONCRETE PAVEMENT IS ENCOUNTERED DURING THE MILLING OPERATION THE CONTRACTOR SHALL NOTIFY THE ENGINEER PRIOR TO PLACING 2.25" OF HMA S0.5".
9. ANY DELAMINATED OR DAMAGED ASPHALT PAVEMENT ENCOUNTERED AFTER THE MILLING OPERATION SHALL BE REPAIRED USING THE ITEM 0601192A - "SURFACE PATCH".
10. ALL MILLED AREAS SHALL BE OVERLAYED WITH 2.25" OF HMA S0.5" (AS SHOWN ON DETAILS) WITHIN FIVE WORKING DAYS (NIGHTS) AS NOTED IN THE MILLING SPECIFICATION. ALL MILLED SURFACES SHALL BE PAVED BY THE END OF THE WORK WEEK.
11. HMA S0.25 FOR WEDGING SHALL BE PLACED WHERE REQUIRED FOR CROSS SLOPE CORRECTION AS INDICATED IN THE CROSS SECTIONS, AS SHOWN ON DETAIL 'WEDGING TO CORRECT CROSS-SLOPE' AND TYPICAL SECTIONS.
12. PLACE 1.5" LIFT OF HMA S0.375 FOLLOWING WEDGING OPERATIONS AS INDICATED ON THE TYPICAL SECTIONS.
13. ALL AREAS THAT HAVE A LONGITUDINAL DROP-OFF OF MORE THAN 2" AT THE EDGE OF PAVEMENT FOLLOWING THE PLACEMENT OF THE 1.5" PAVEMENT LIFT SHALL BE BACKFILLED WITH SUBBASE MATERIAL (SEE DETAIL- WEDGING TO CORRECT CROSS-SLOPE). THIS WORK SHALL BE PAID FOR UNDER CONTRACT ITEM "MAINTENACE AND PROTECTION OF TRAFFIC".
14. THE CONTRACTOR MUST INSTALL TEMPORARY PAVEMENT MARKINGS AND SIGNS THROUGHOUT THE WORK AREA FOLLOWING THE THE COMPLETION OF ANY MILLING AND/OR PAVING OPERATION AND PRIOR TO OPENING THE WORK AREA TO TRAFFIC IN ACCORDANCE WITH D.O.T. STANDARD SHEET M1209-A AND THE CONTRACT SPECIAL PROVISION FOR ITEM NO. 0971001A.
15. AFTER THE INSTALLATION OF THE 1.5" LIFT OF S0.375, THE CONTRACTOR SHALL PERFORM A SURVEY OF THE PAVED ROADWAY TO VERIFY CROSS-SLOPES AS DIRECTED UNDER SPECIAL PROVISION NO. 0980001A - CONSTRUCTION STAKING FOR APPROVAL BY THE ENGINEER.

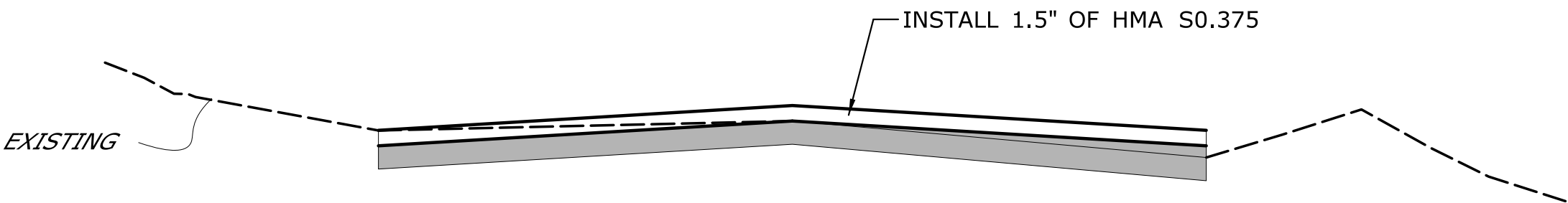
STAGE 2 - CROSS-SLOPE CORRECTION
SUGGESTED SEQUENCE OF OPERATIONS



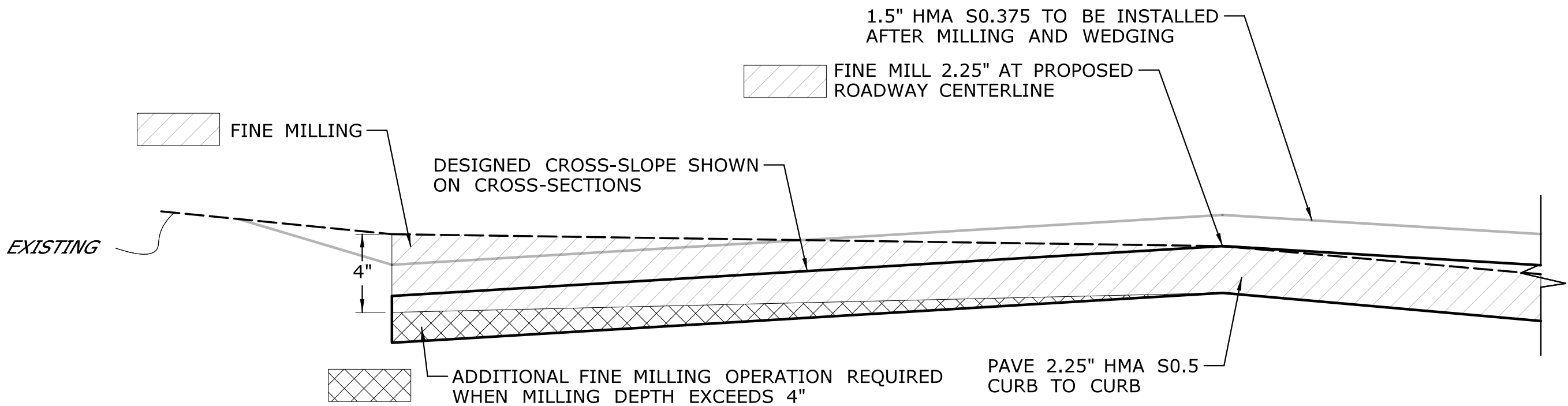
FINE MILLING A MINIMUM OF 2.25" OF EXISTING PAVEMENT
AND FINE MILLING TO CORRECT CROSS-SLOPE



USE HMA S0.25 TO INSTALL WEDGE COURSE
TO CORRECT CROSS-SLOPE

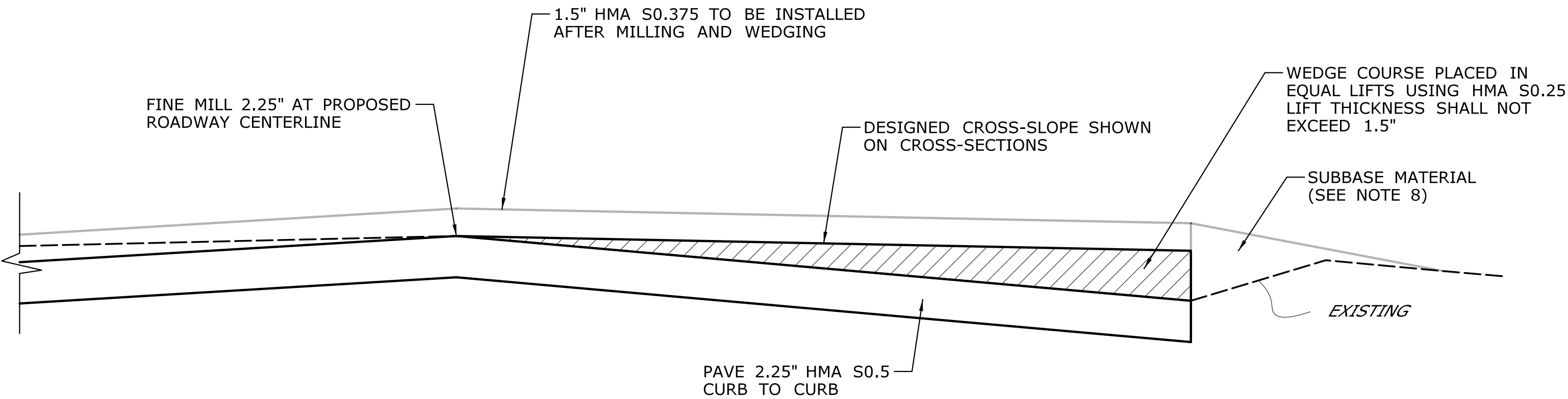


PAVE 1.5" OF HMA S0.375



FINE MILLING TO CORRECT CROSS-SLOPE DETAIL

NOTE: ADDITIONAL MILLING PASS TO BE PAID FOR AT THE CONTRACT UNIT PRICE FOR "FINE MILLING OF BITUMINOUS CONCRETE (0-4")"



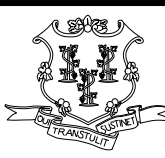
WEDGING TO CORRECT CROSS-SLOPE DETAIL


-	-	-	-
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REV.	DATE	REVISION DESCRIPTION	SHEET NO.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.


Plotted Date: 7/18/2016

DESIGNER/DRAFTER:
MSC
CHECKED BY:
MSC
NOT TO SCALE

**STATE OF CONNECTICUT**
DEPARTMENT OF TRANSPORTATION



Filename: ...\\158-0211-StagingPlan-STG-02.dgn

SIGNATURE/
BLOCK:
OFFICE OF ENGINEERING
APPROVED BY:  DATE:

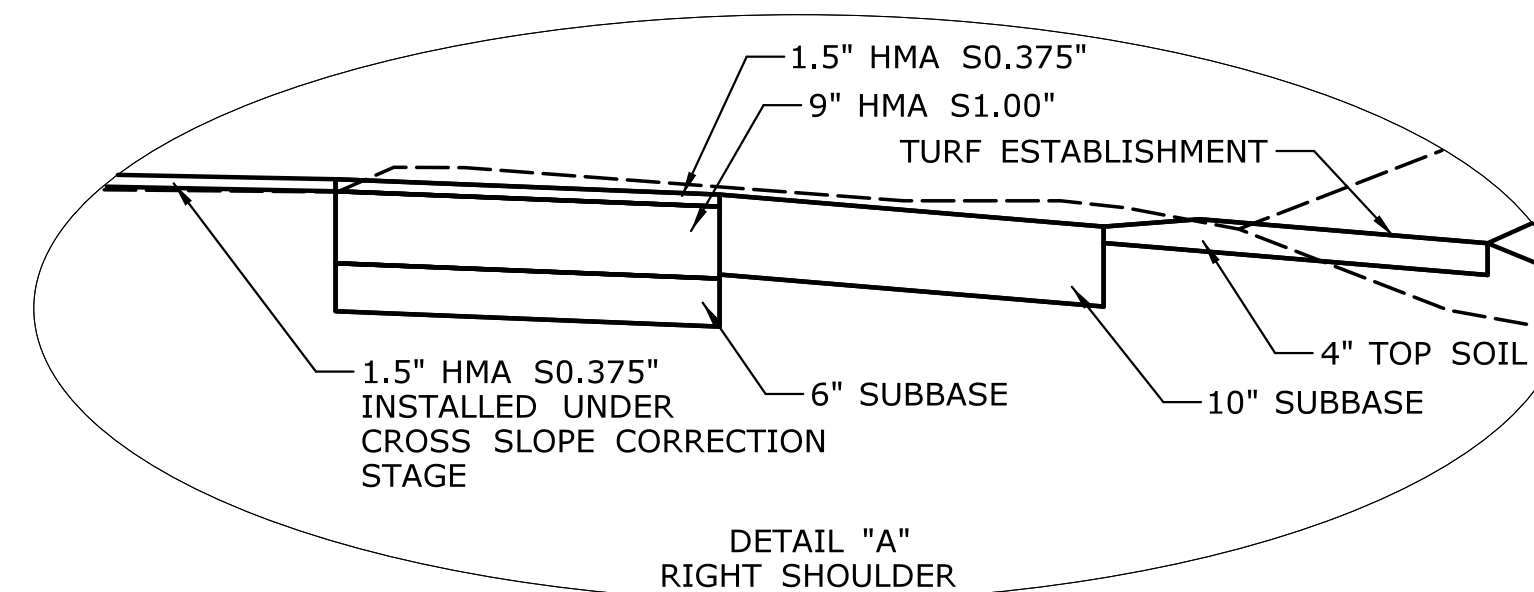
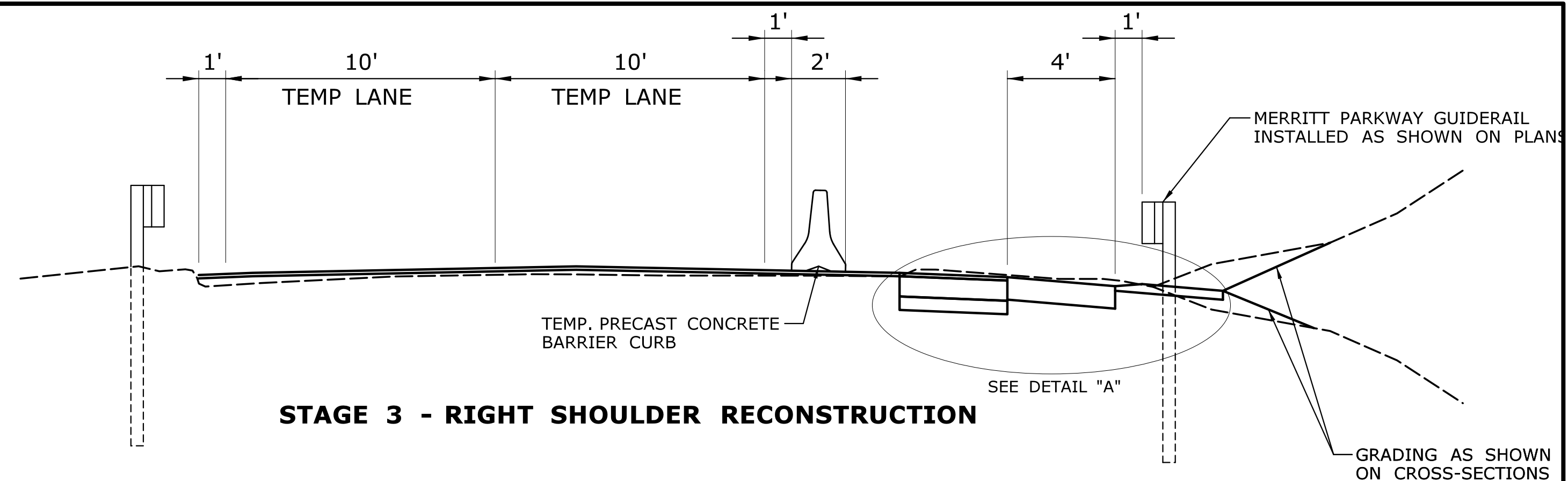
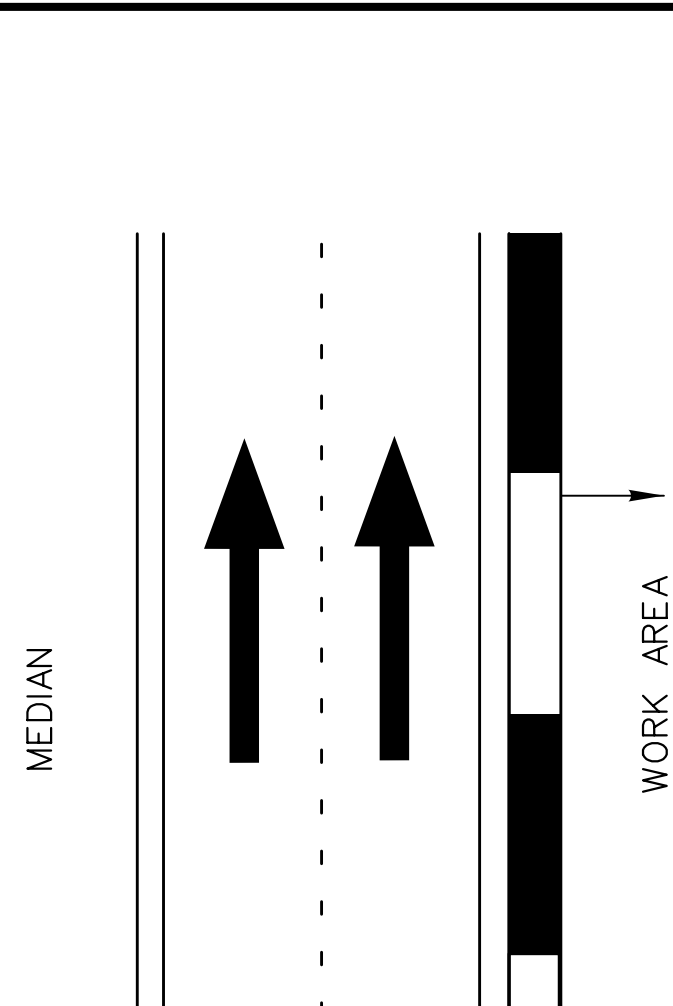
PROJECT TITLE:
**ROUTE 15 SAFETY IMPROVEMENTS,
RESURFACING, ENHANCEMENTS,
AND BRIDGE IMPROVEMENTS**

TOWN:
**WESTPORT
FAIRFIELD**
DRAWING TITLE:
**STAGE CONSTRUCTION
PLANS - STAGE 2**

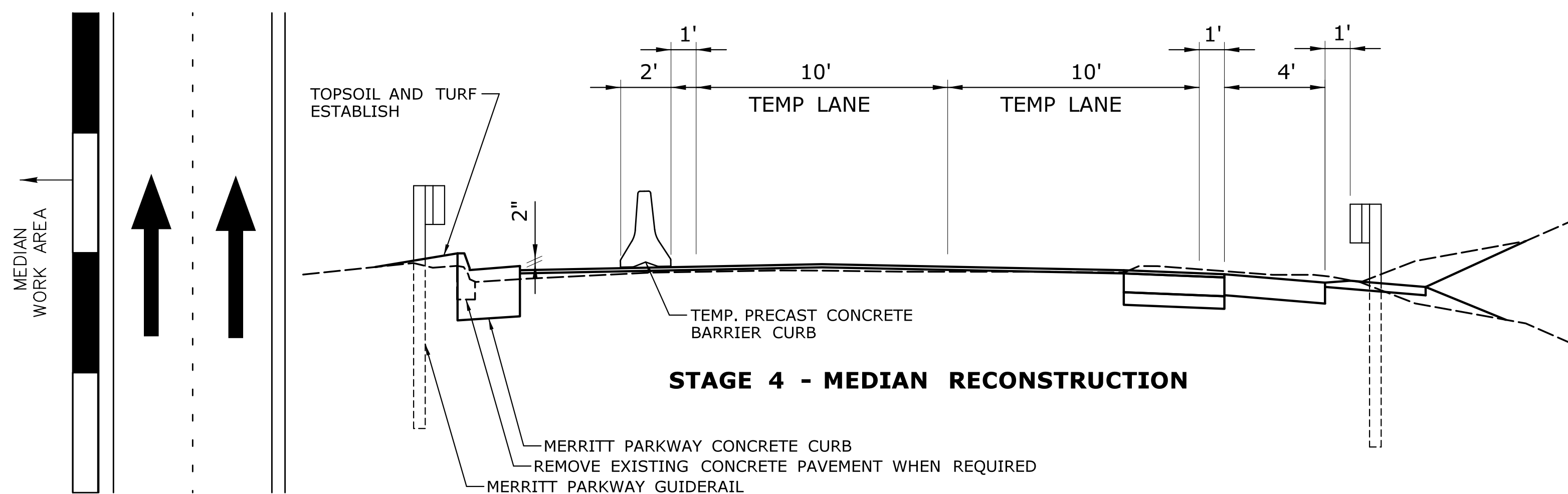
PROJECT NO.
158-0211
DRAWING NO.
STG-02
SHEET NO.
01.03.06

1. ALL WORK IN STAGE 3 SHALL BE COMPLETED BEFORE ANY WORK IN STAGE 4 IS STARTED.
2. ALL WORK IN STAGE 4 SHALL BE COMPLETED BEFORE ANY WORK IN STAGE 5 IS STARTED.
3. THE CONTRACTOR SHALL BE ALLOWED TO MAINTAIN TWO ACTIVE WORK AREAS CONCURRENTLY IN EACH DIRECTION. WORK AREAS SHALL NOT EXCEED A LENGTH OF 1.5 MILES AND SHALL BE A MINIMUM DISTANCE OF 1 MILE APART. ANY MINOR STAGE CONSTRUCTION WORK AREAS, AS DETERMINED BY THE ENGINEER, CAN BE CONCURRENT WITH STAGE 3 OR 4 AS APPROVED BY THE ENGINEER.
4. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PRESERVE ANY TREES TO REMAIN. THIS SHALL INCLUDE THE PRESERVATION OF THE TREE'S ROOT SYSTEMS.
5. ALL ROCK EXCAVATION THAT REQUIRES THE USE OF EXPLOSIVES SHALL BE PRE- SPLIT IN ACCORDANCE WITH FORM 816. HOWEVER, NOT ALL ROCK REMOVAL CAN BE ACCOMPLISHED WITH EXPLOSIVES.
6. THE CONTRACTOR SHALL NOT BE PAID FOR REMOVING AND RESETTING TEMPORARY PRECAST CONCRETE BARRIER CURB FOR ACCESS TO WORK AREA. IT SHALL BE INCLUDED IN GENERAL COST OF WORK.
7. LANE AND SHOULDER CLOSURES SHALL BE IMPLEMENTED DURING ALLOWABLE PERIODS AS DICTATED IN THE SPECIAL PROVISION FOR SECTION 1.08.04 AND IN ACCORDANCE WITH THE SPECIAL PROVISION FOR ITEM NO. 0971001A.
8. THE INSTALLATION OF THE CONCRETE CURB AND GUTTER SECTION WILL BE LIMITED IN ACCORDANCE WITH LIMITATION OF OPERATIONS SPECIFIED IN THE CONTRACT SPECIAL PROVISIONS.
9. TEMPORARY PRECAST CONCRETE BARRIER CURB SHALL HAVE APPROPRIATE DELINEATORS INSTALLED ON THEM PER D.O.T. STANDARD SHEET M1205-A.
10. TYPICAL M&PT SIGNING AND PAVEMENT MARKING PLANS WILL BE INCLUDED IN TRAFFIC ENGINEERING'S SPECIAL PROVISION FOR ITEM NO. 0971001A.

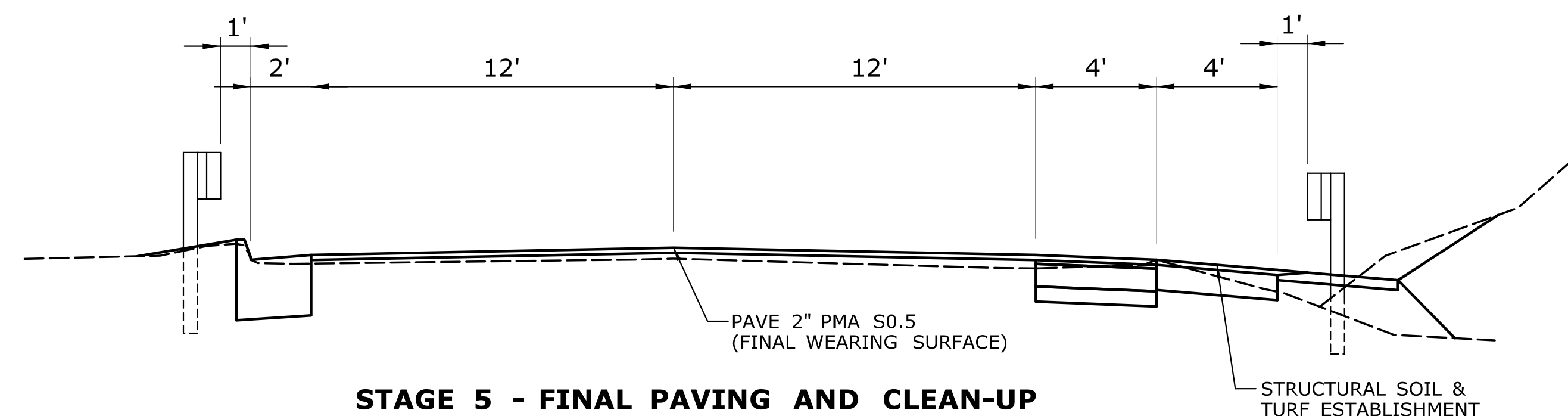
1. INSTALL TEMPORARY PAVEMENT MARKINGS, SHIFT TRAFFIC TO THE LEFT (MEDIAN)SIDE OF THE ROADWAY, AND CONSTRUCT RIGHT SHOULDER.
2. THE PAVED SHOULDER SHALL BE CONSTRUCTED UP TO AND INCLUDING THE 1.5" LIFT OF HMA S0.375. PAVEMENT MUST BE INSTALLED BY APPROVED MECHANICAL MEANS. PAVEMENT INSTALLATION BY HAND SHALL NOT BE ALLOWED UNLESS APPROVED BY THE ENGINEER AND IS IN ACCORDANCE WITH SECTION 4.06 OF FORM 816.
3. CONSTRUCT REINFORCED GRASS SHOULDER TO STAGE 3 PAVEMENT ELEVATION (SUBBASE ONLY).
4. ALL EXCAVATION INCLUDING ROCK EXCAVATION AND PLACING OF FILL SHALL BE COMPLETED.
5. PROPOSED DRAINAGE STRUCTURES LOCATED WITHIN THE EDGE OF PAVEMENT SHALL BE INSTALLED TO THE ELEVATION OF THE 1.5" HMA S0.375 LIFT. PROPOSED DRAINAGE STRUCTURES LOCATED OUTSIDE THE EDGE OF PAVEMENT SHALL BE INSTALLED TO FINAL GRADE.
6. CLEAN DRAINAGE OUTLETS USING THE ITEMS INCLUDED IN THE CONTRACT AS DIRECTED BY THE ENGINEER.
7. INSTALL GUIDERAIL TO FINAL GRADE.
8. INSTALL PLANTINGS BEHIND GUIDERAIL.
9. TURF ESTABLISH AS SHOWN IN DETAILS.



1. INSTALL TEMPORARY PAVEMENT MARKINGS AND SHIFT TRAFFIC TO THE RIGHT SIDE OF THE ROADWAY.
2. REMOVE CONCRETE PAVEMENT IF REQUIRED.
3. CONSTRUCT LEFT CURB AND GUTTER SECTION TO FINAL GRADE.
4. INSTALL TYPE "CM-2" CATCH BASINS TO THE ELEVATION OF THE 1.5" HMA SO.375 LIFT. INSTALL ALL REMAINING MEDIAN DRAINAGE TO FINAL GRADE. PLACE TRAFFIC DRUMS ON TYPE "C" CATCH BASINS AND ACCOMMODATE FOR TEMPORARY DRAINAGE (SEE TYPE "C" MEDIAN CATCH BASIN TEMPORARY DRAINAGE TREATMENT).
5. INSTALL GUIDERAIL AND MEDIAN BARRIER.
6. INSTALL PLANTINGS IN MEDIAN AND TURF ESTABLISH AS SHOWN IN THE DETAILS.



1. ALL STAGE 3 & 4 WORK SHALL BE COMPLETE PRIOR TO BEGINNING STAGE 5 WORK. THIS STAGE MUST BE ONE CONTINUOUS OPERATION FROM BEGINNING TO THE END OF THE PROJECT IN EACH DIRECTION.
2. ALL DRAINAGE STRUCTURES INSTALLED AT THE 1.5" HMA S0.375 ELEVATION AND ANY ADDITIONAL DRAINAGE STRUCTURES REQUIRING RESETS MUST BE RESET TO FINAL GRADE PRIOR TO FINAL PAVEMENT OPERATIONS. THE RESETTING OF TYPE "CM-2" BASINS SHALL BE PAID FOR UNDER THE ITEM "RESET TYPE "C-M-2" CATCH BASIN."
3. PLACE 2" LIFT OF PMA S0.5 AS INDICATED ON CROSS SECTIONS.
4. MILL AND PAVE RAMPS AS INDICATED IN THE TYPICAL SECTIONS.
5. ELIMINATE TEMPORARY DRAINAGE TREATMENTS (SEE DETAIL).
6. INSTALL REMAINING PLANTINGS.
7. INSTALL FINAL PAVEMENT MARKING.
8. PLACE TOPSOIL AND STRUCTURAL SOIL & ESTABLISH TURF TO FINAL GRADE.
9. CLEAN ALL REMAINING DRAINAGE STRUCTURES AS DIRECTED BY THE ENGINEER AND PAID FOR UNDER THOSE ITEMS INCLUDED IN SECTION 6.53 OF FORM 816.



"	"	"	"	"
"	"	"	"	"
"	"	"	"	"
"	"	"	"	"
"	"	"	"	"
REV.	DATE	REVISION	DESCRIPTION	SHEET NO.

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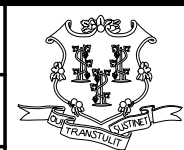
Plotted Date: 7/18/2016

DESIGNER/DRAFTER:

MSC

CHECKED BY: _____

SCALE AS NOTED



STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION



Filename: ...\\158-0211-StagingPlan-STG-03.dgn

SIGNATURE/ BLOCK:	
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OFFICE OF ENGINEERING

APPROVED BY:

DATE:

Willi Barthel

PROJECT TITLE:

ROUTE 15 SAFETY IMPROVEMENTS, RESURFACING, ENHANCEMENTS, AND BRIDGE IMPROVEMENTS

TOWN:

WESTPORT FAIRFIELD

DRAWING TITLE:

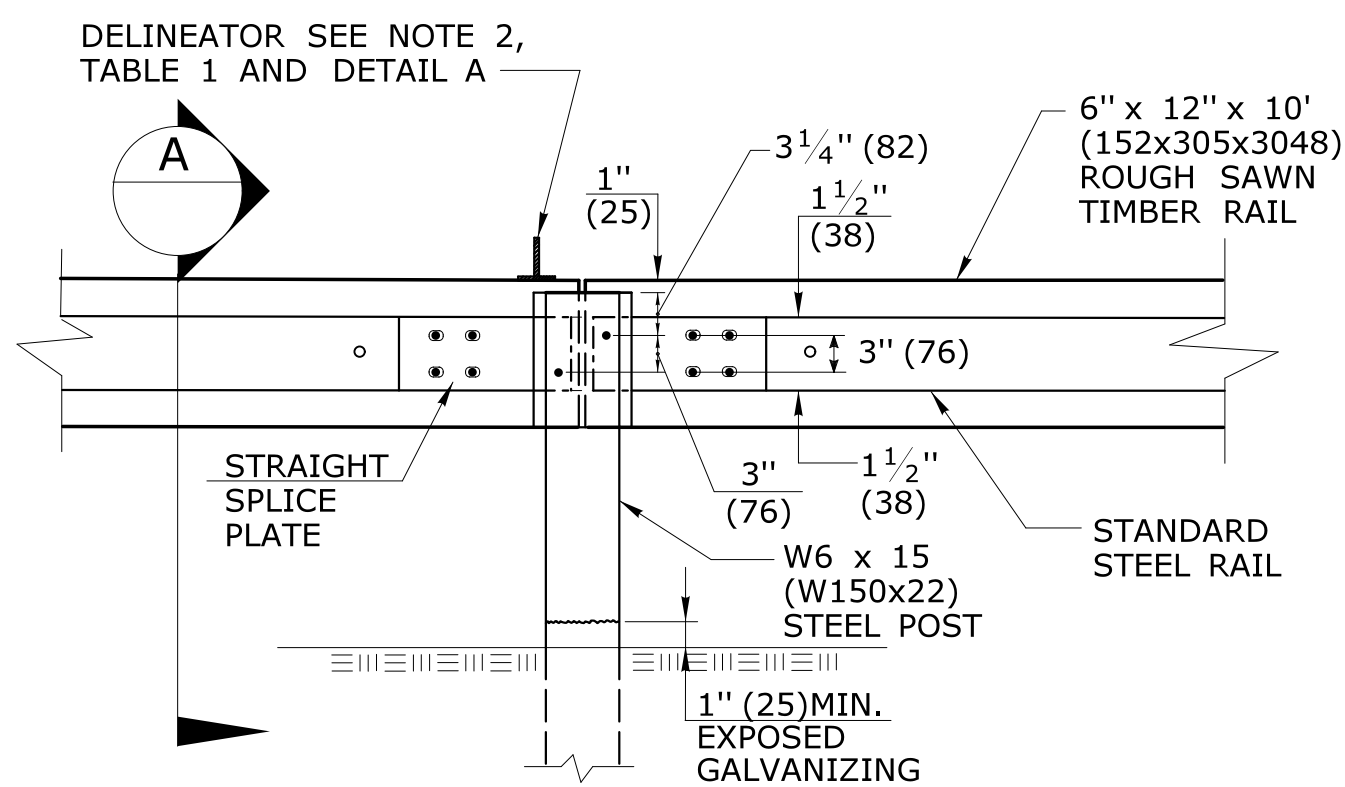
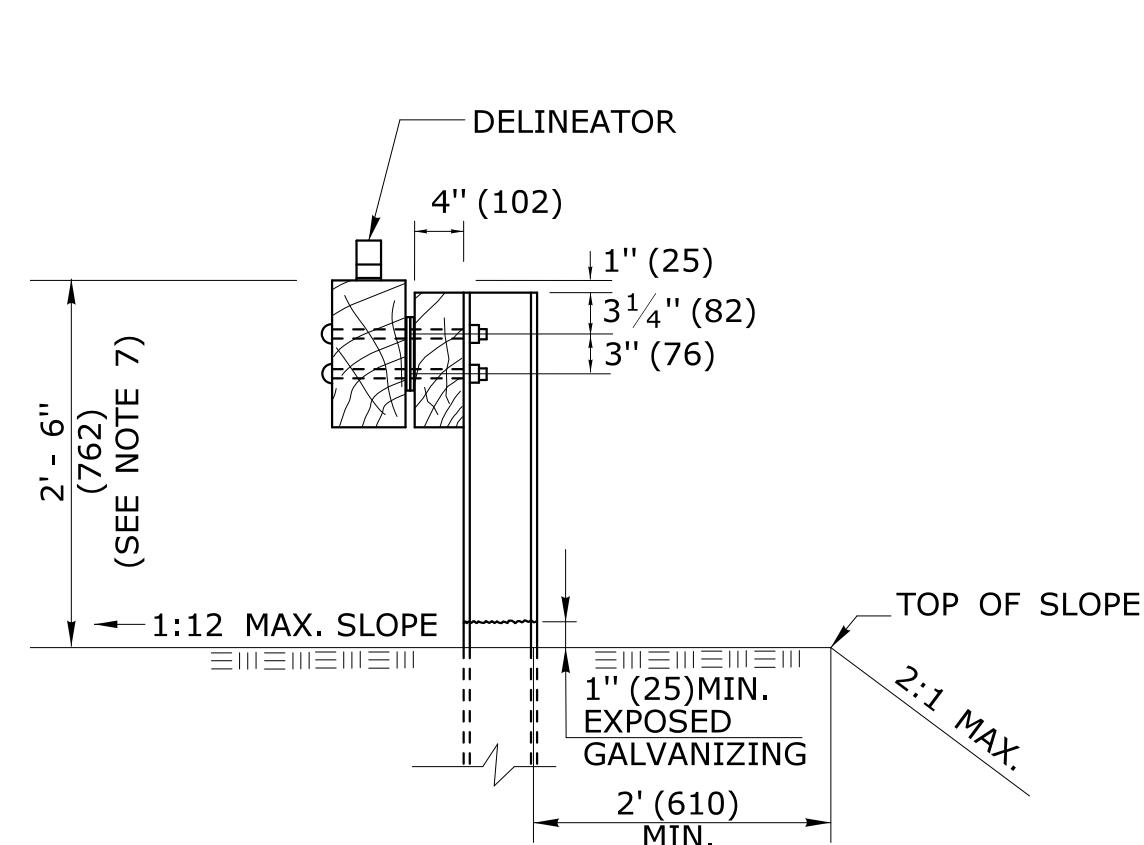
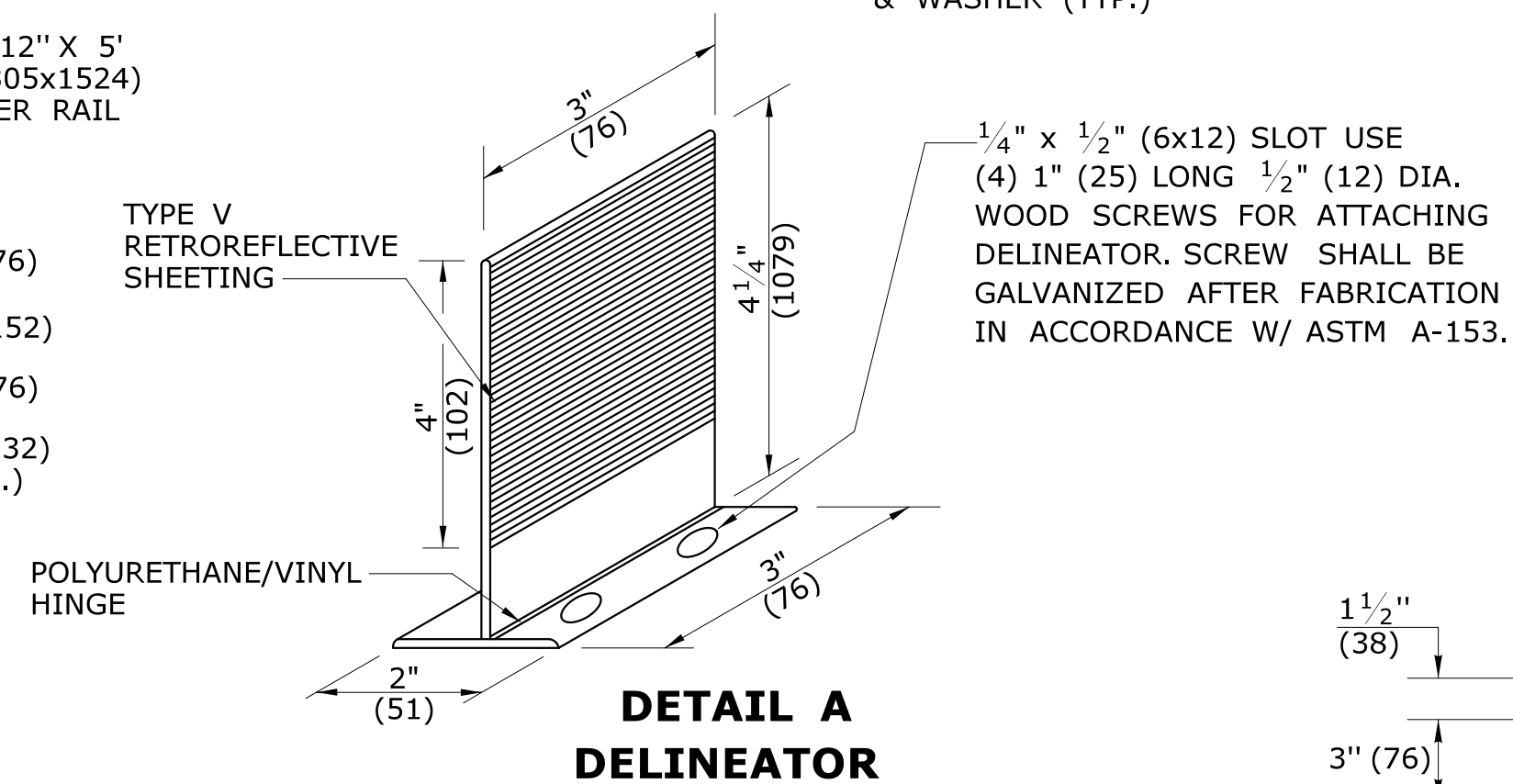
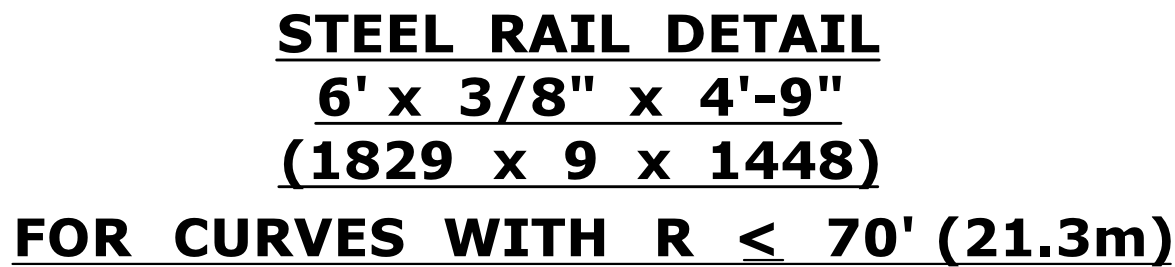
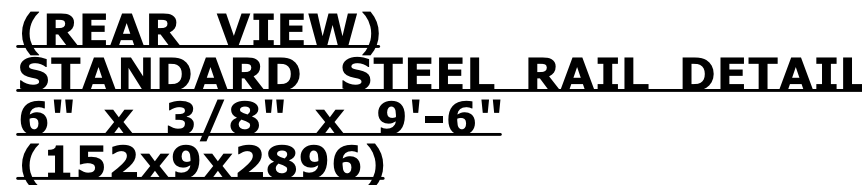
STAGE CONSTRUCTION STAGES 3, 4 & 5

PROJECT NO.	
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158-0211

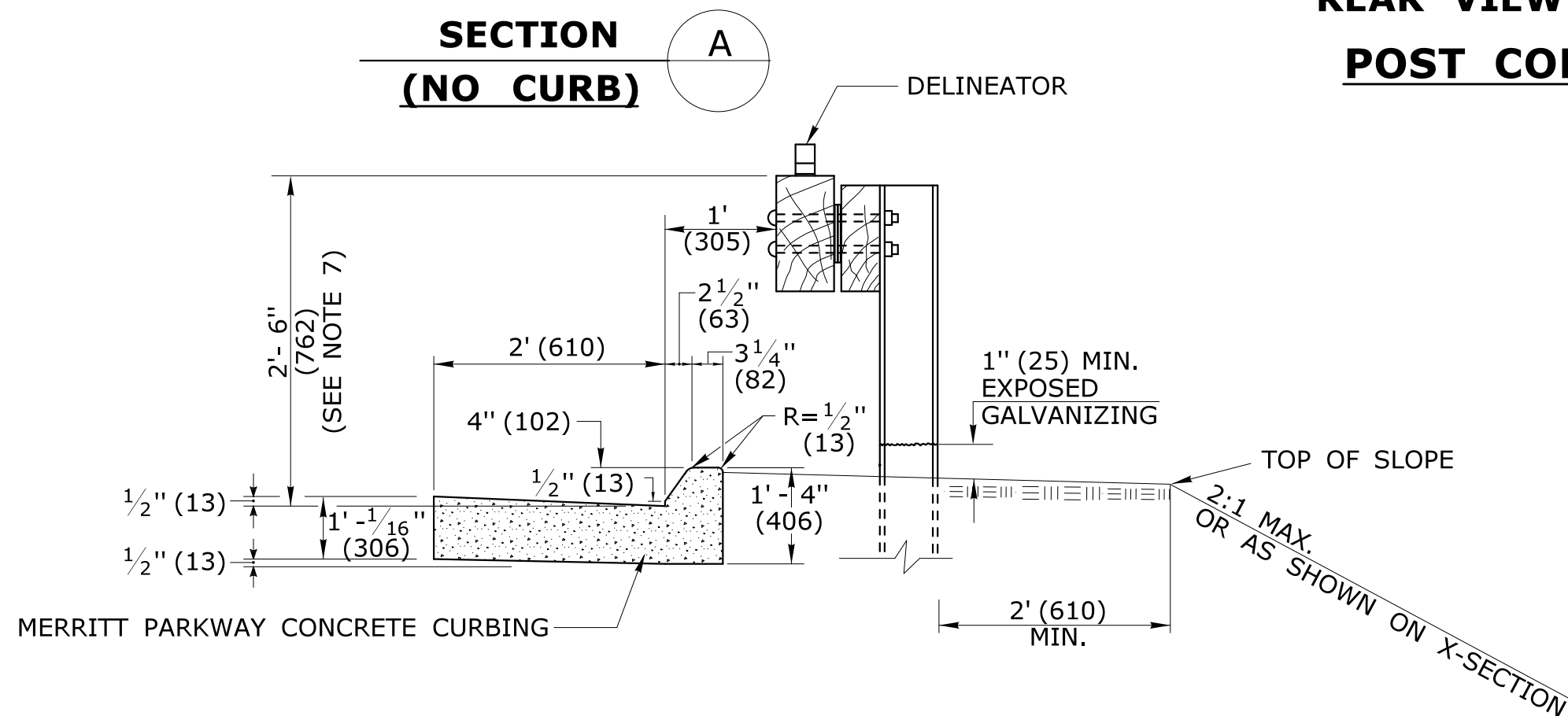
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STG-03

SHEET NO.
01.03.07

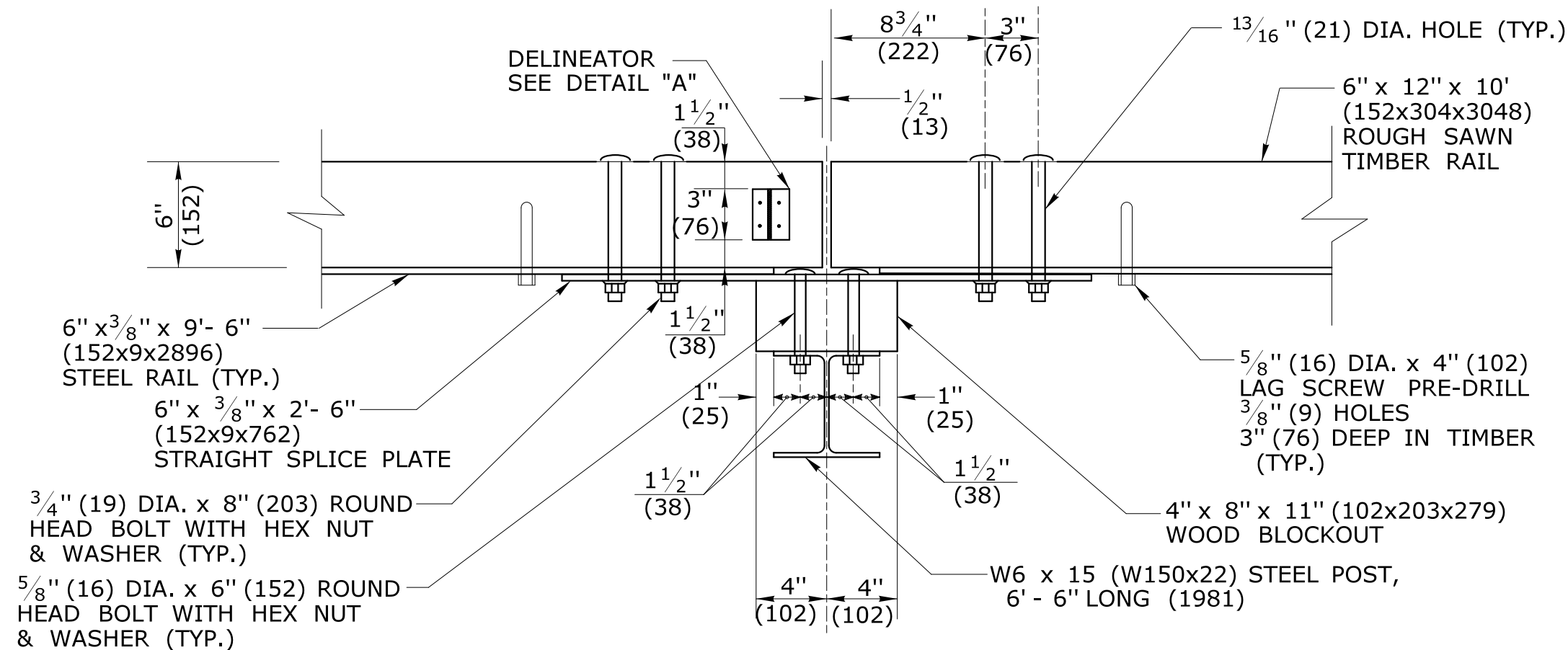


**REAR VIEW ELEVATION
POST CONNECTION**

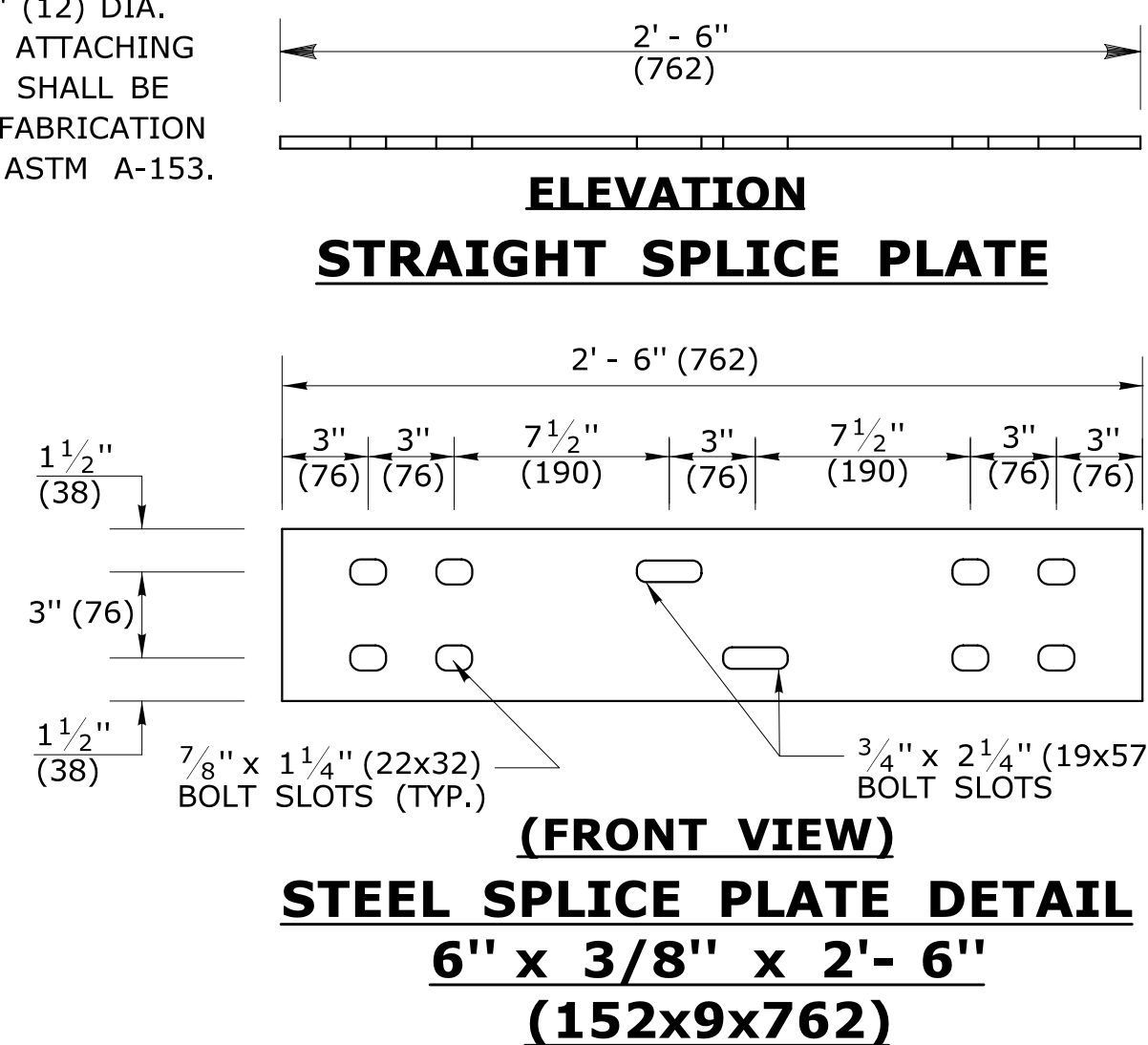
DELINEATOR SPACING	
CURVE RADIUS	SPACE
< 300' (91.4m)	20' (6.10m)
≥ 300' (91.4m)	50' (15.2m)



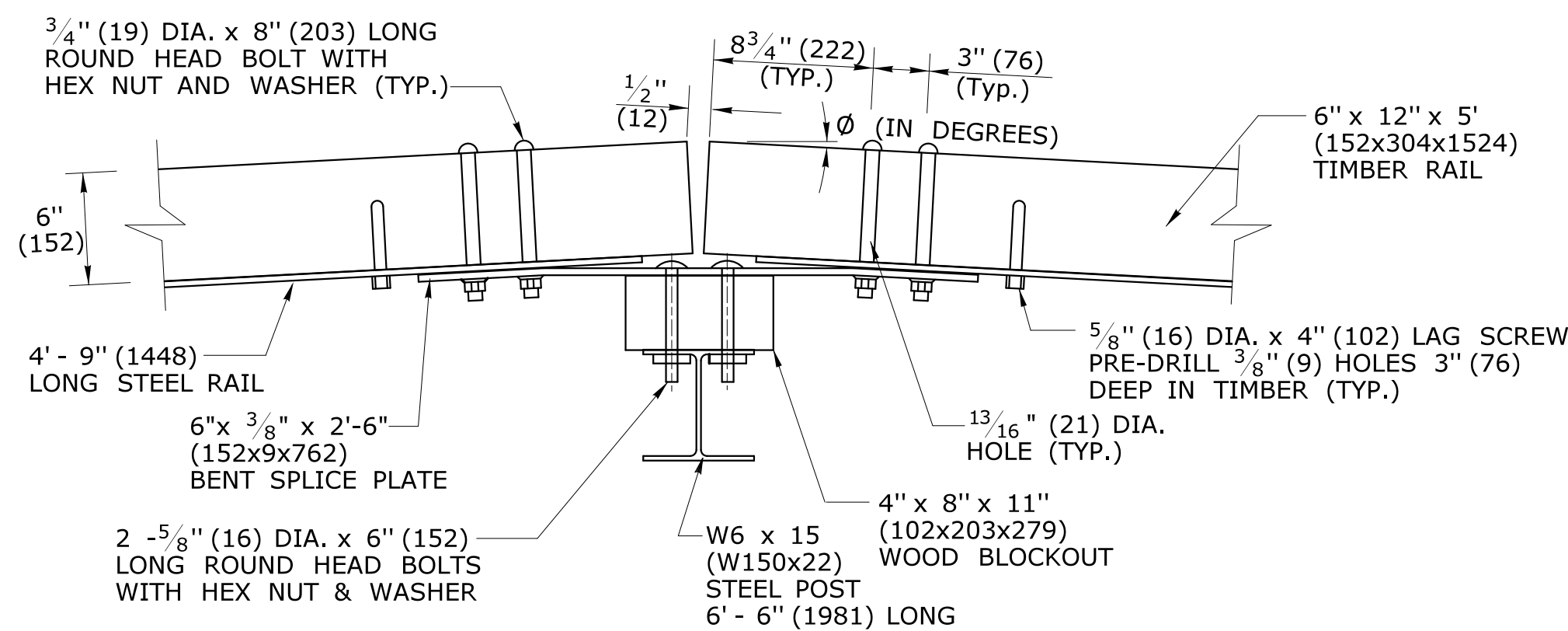
MERRITT PARKWAY CONCRETE CURBING WITH MERRITT PARKWAY GUIDERAIL



POST CONNECTION FOR STRAIGHT SECTIONS

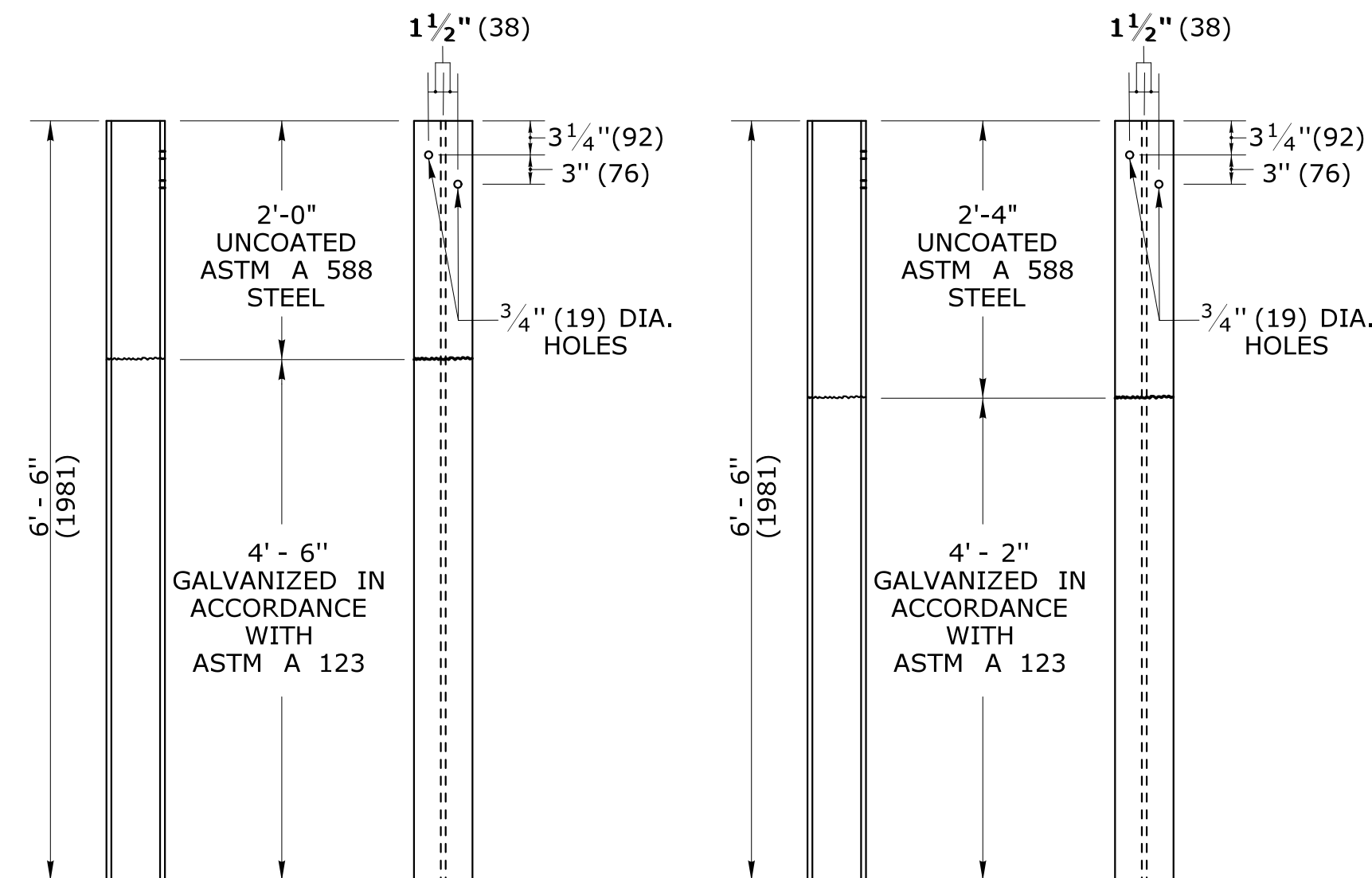


ELEVATION
BENT SPLICE PLATE



POST CONNECTION FOR CURVES WITH $R < 70'$ (21.3m)

1. MATERIALS AND CONSTRUCTION OF THE MERRITT PARKWAY GUIDERAIL SHALL CONFORM TO SPECIAL PROVISION PROVIDED WITH THE PROJECT.
2. Delineators shall be plastic inverted T-sections in accordance with detail "A". Reflectors shall be spaced in accordance with Table 1, and positioned perpendicular to the adjacent edge of lane. Do not attach reflectors on flare or terminal sections. Reflective sheeting shall be silver-white on all rail sections adjacent to the right shoulder, and yellow on rail sections adjacent to the left shoulder of travel lanes. Delineators shall be included in the cost for "Merritt Parkway Guiderail."
3. Two additional $\frac{5}{8}$ " (16) DIA. X 4"(102) long lag screws and washers shall be installed at all mid-span points for standard systems.
4. All connection hardware shall be sufficiently tightened to accommodate for shrinkage of the wood elements.
5. When furnishing the shop bent splice plates, use the minimum bend angle as shown in Table 2.
6. For railing set on a curve with $R_{DII} < 70'$ (21.3m) space posts at 5' (1524).
7. Guiderail height may vary up to 2" plus or minus the height shown in the details to achieve a smooth and continuous guiderail profile.



SIDE VIEW FRONT VIEW
POST DETAIL
W6 X 15
(W150x22)
WITH CURBING

SIDE VIEW FRONT VIEW

POST DETAIL

W6 X 15



(W150x22)

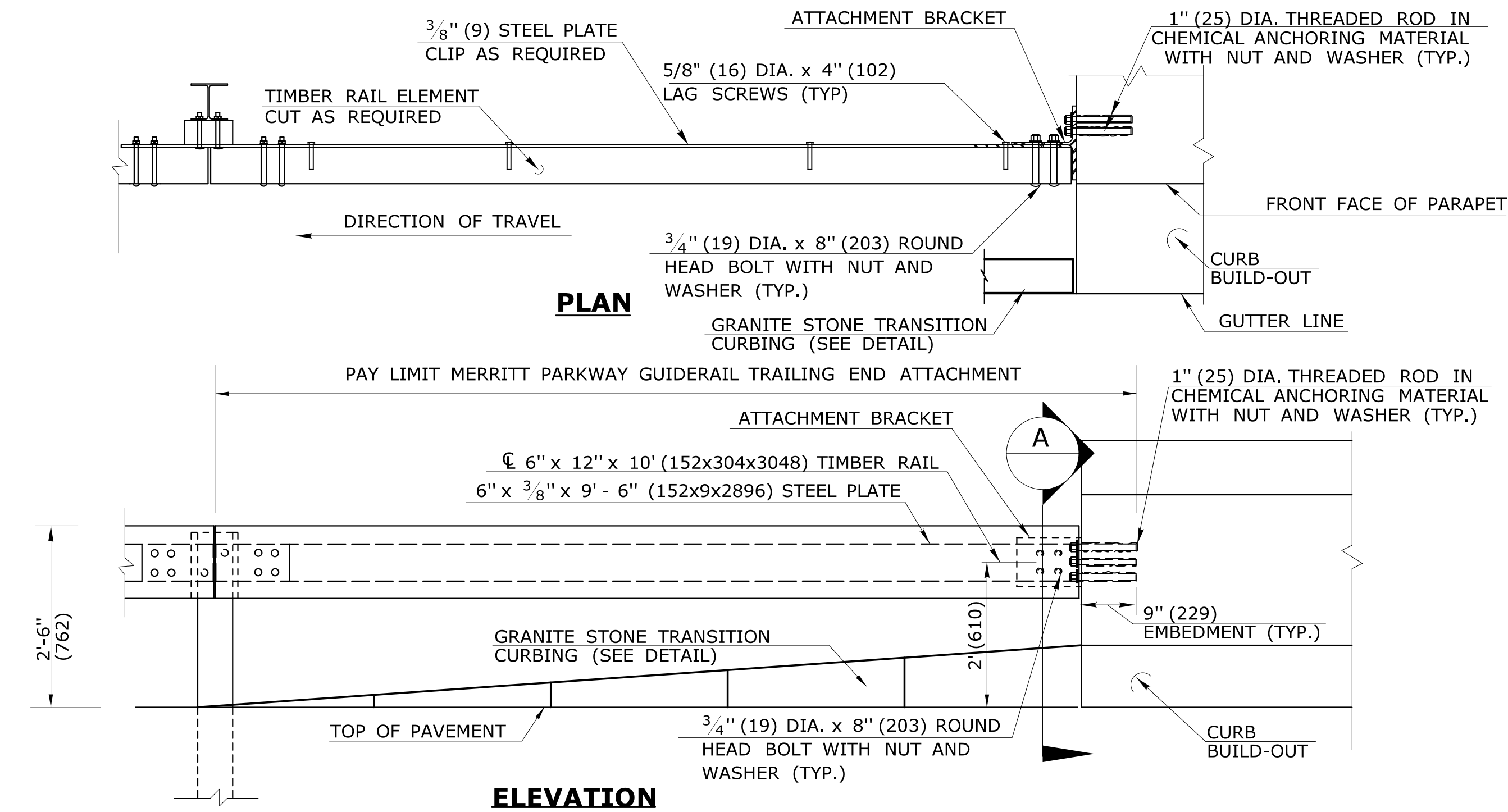
NO CURBING

TABLE 2

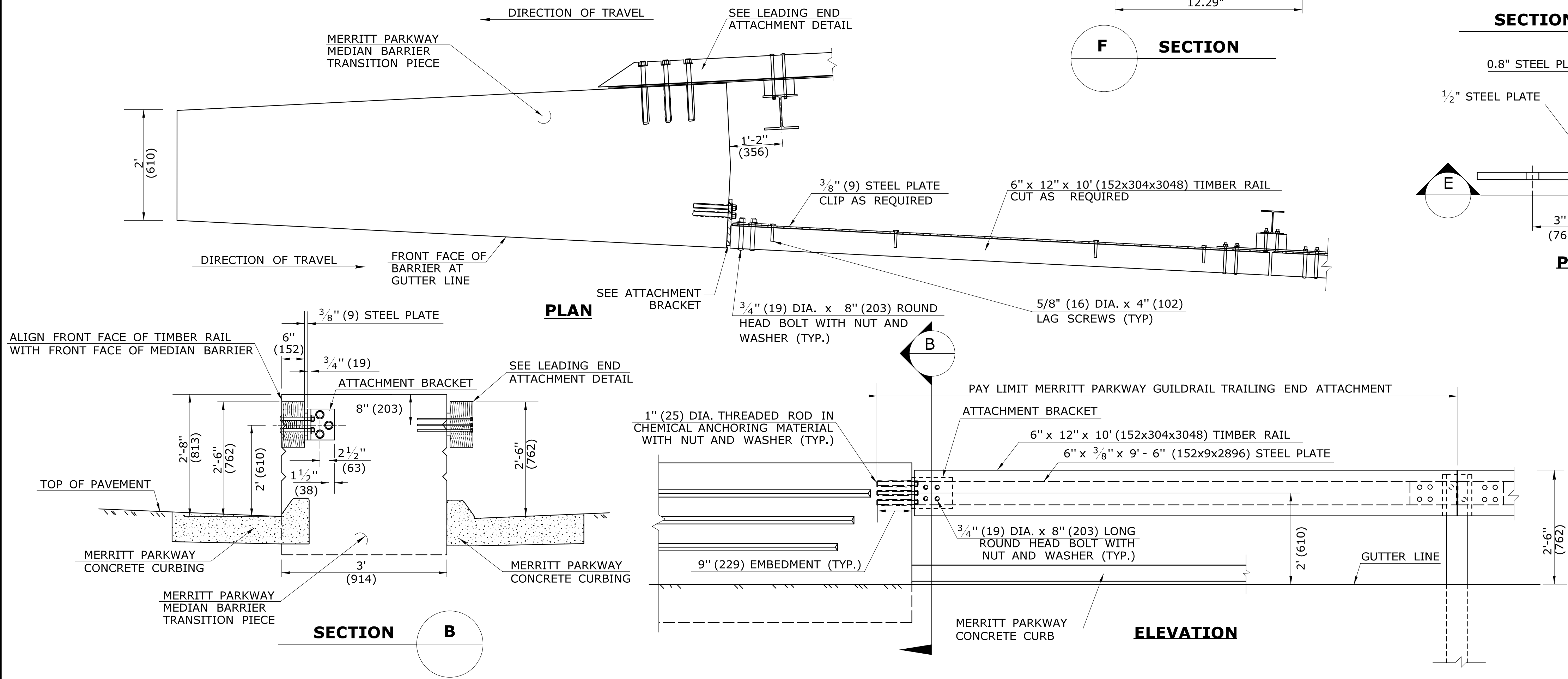
Radius R (ft.)(m)	Ø (Degrees)	d (in.)(mm)
35 (10.7)Min.	4.10	$\frac{5}{8}$ (16)
40 (12.2)	3.58	$\frac{9}{16}$ (14)
45 (13.7)	3.18	$\frac{1}{2}$ (13)
50 (15.2)	2.86	$\frac{7}{16}$ (11)
55 (16.8)	2.60	$\frac{7}{16}$ (11)
60 (18.3)	2.40	$\frac{3}{8}$ (9)
65 (19.8)	2.20	$\frac{3}{8}$ (9)
70 (21.3)	2.05	$\frac{5}{16}$ (8)
Over 70 (21.3)	Flat	0

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

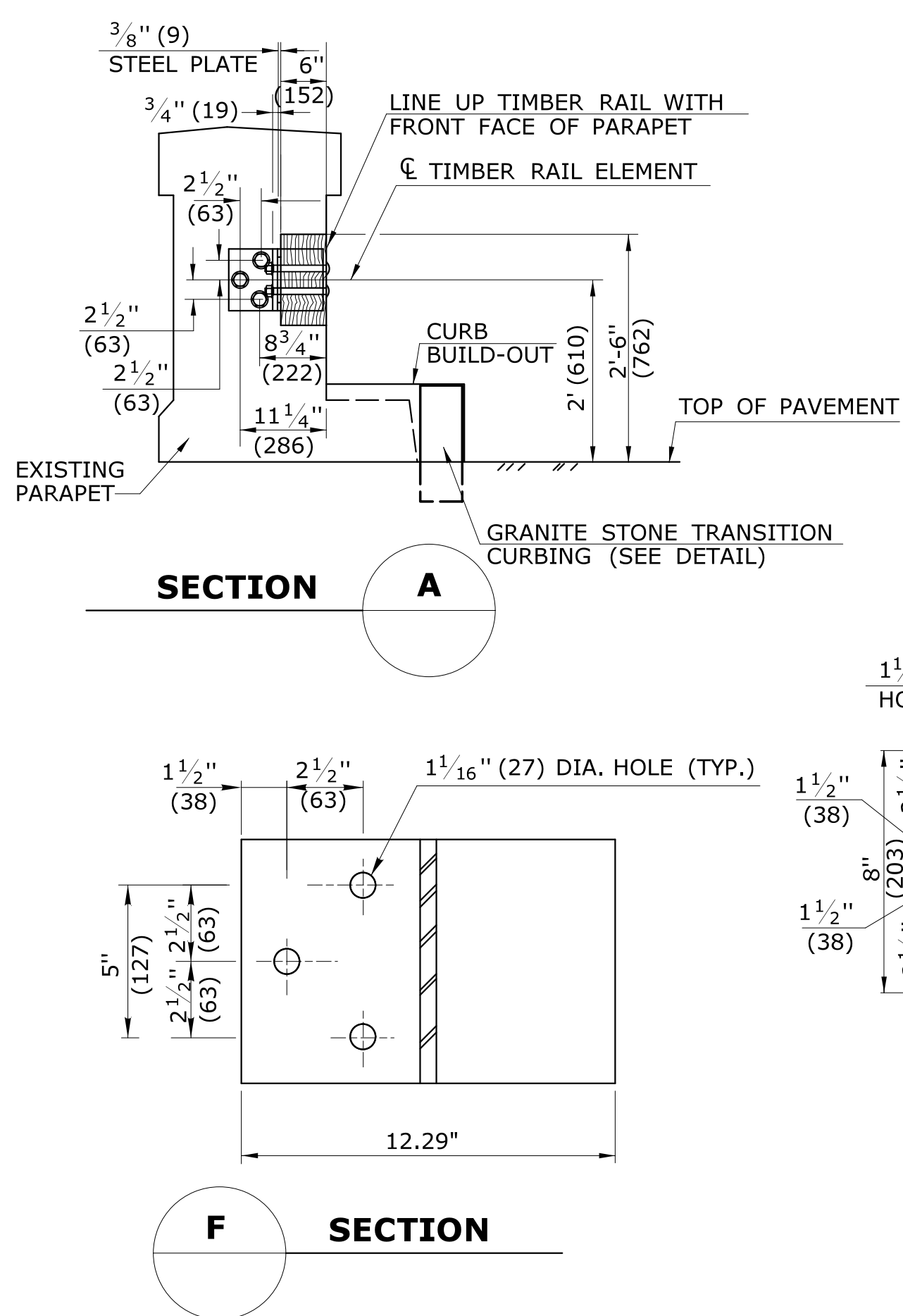
					THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		DESIGNER/DRAFTER: - CHECKED BY: - SCALE AS NOTED		<div>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</div> <div>Filename: ...\\158-211_Guide-01.MPGR...dgn</div>		SIGNATURE/ BLOCK: APPROVED BY: <div></div>		PROJECT TITLE: ROUTE 15 SAFETY IMPROVEMENTS, RESURFACING, ENHANCEMENTS, AND BRIDGE IMPROVEMENTS		TOWN: WESTPORT FAIRFIELD DRAWING TITLE: MERRITT PARKWAY GUIDERAIL		PROJECT NO. 158-211 DRAWING NO. GDS-01 SHEET NO. 01.03.08	
REV.	DATE	REVISION	DESCRIPTION	SHEET NO.	Plotted Date: 7/18/2016													



TRAILING END ATTACHMENT AT EXISTING BRIDGE PARAPET WITH CURB BUILD-OUT

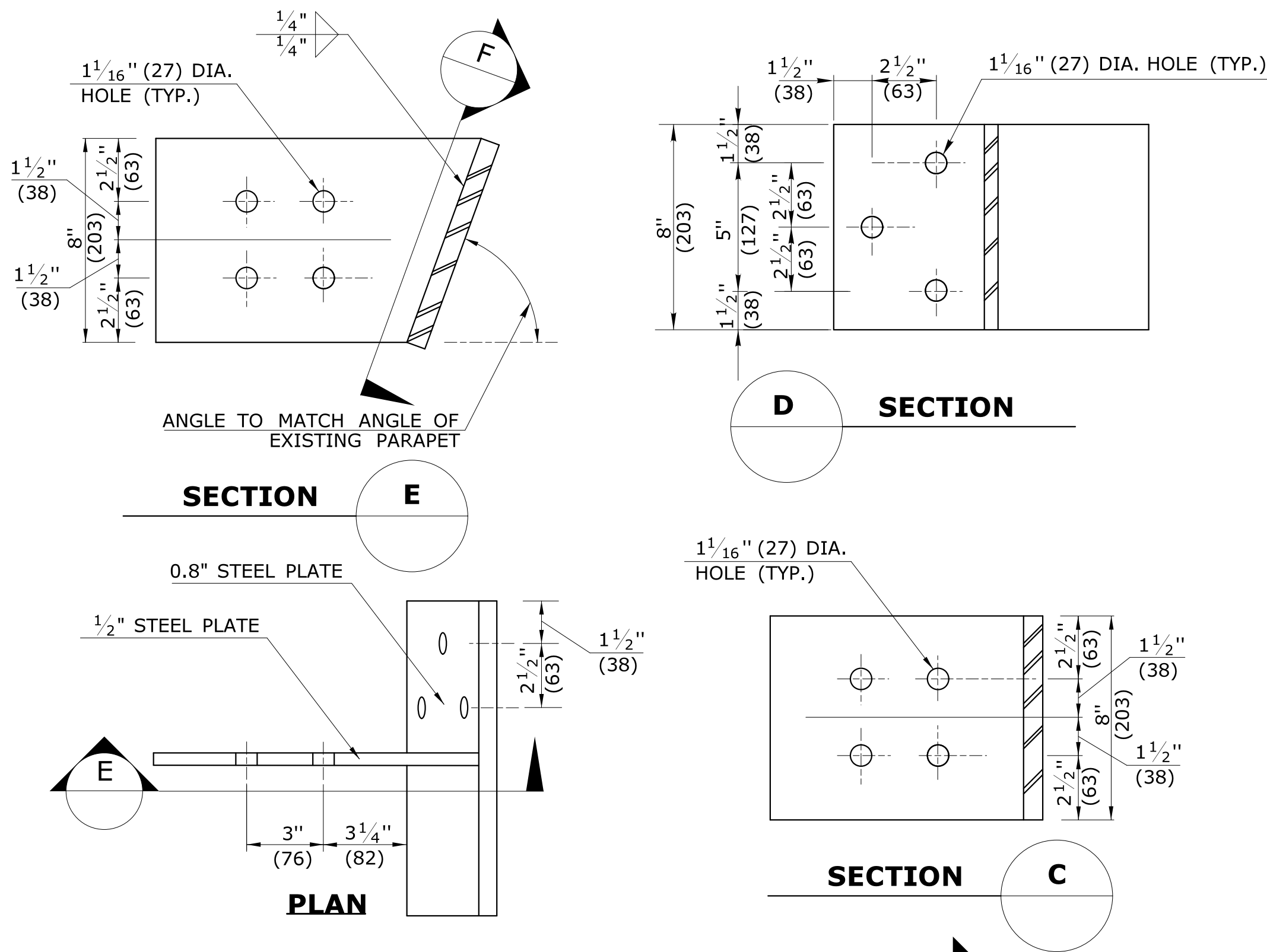


MPGR ATTACHMENTS AT MERRITT PARKWAY MEDIAN BARRIER

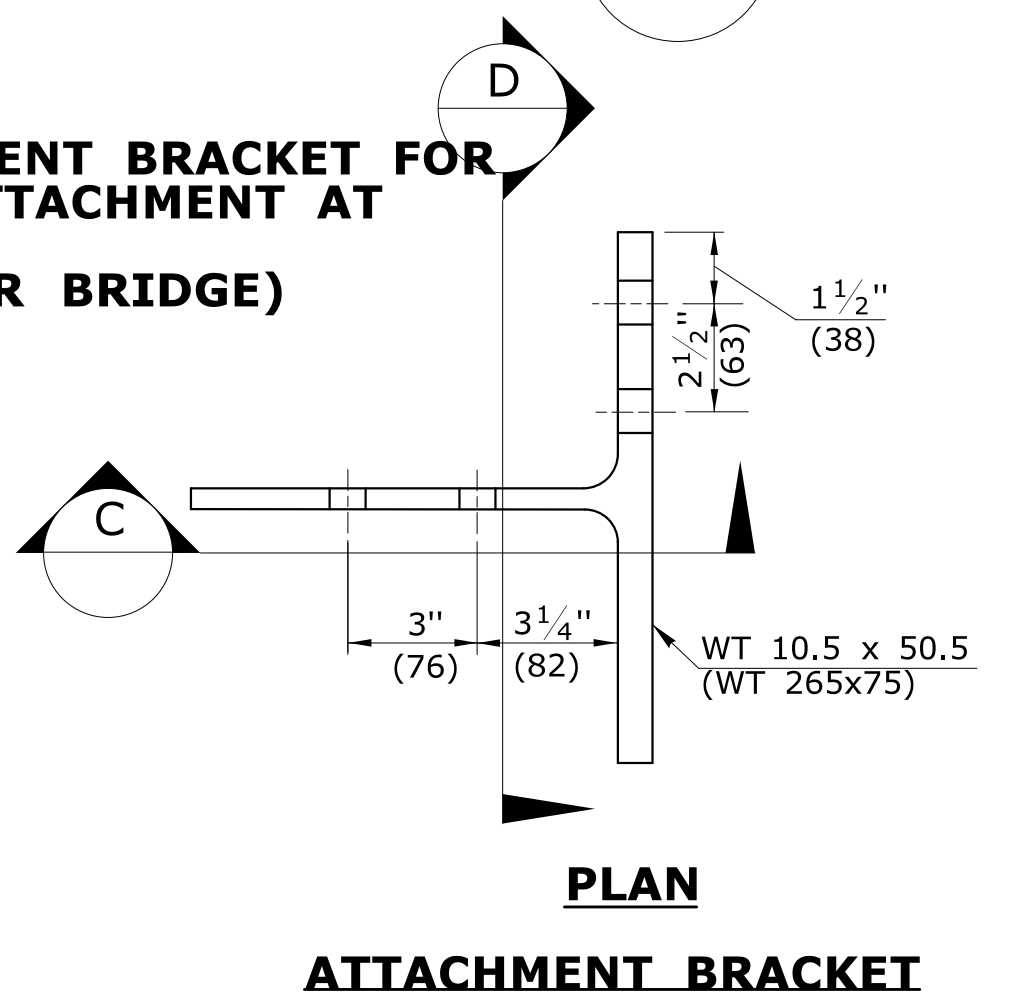


GENERAL NOTES:

1. THREADED RODS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A325, WITH NUTS CONFORMING TO ASTM A563, GRADE C. CIRCULAR HARDENED WASHERS SHALL CONFORM TO ASTM F436. THREADED NUTS AND WASHERS IN CONTACT WITH CONCRETE SHALL BE GALVANIZED IN ACCORDANCE WITH ASTM A153. ROUND HEAD BOLTS SHALL CONFORM TO THE SPECIAL PROVISIONS PROVIDED WITH THE PROJECT.
2. CHEMICAL ANCHORING MATERIAL SHALL CONFORM TO SECTION M.03, SUBARTICLE M.03.01-15
3. ATTACHMENT BRACKETS SHALL CONFORM TO ASTM A36 AND BE GALVANIZED IN ACCORDANCE WITH ASTM A123.
4. THE PAY LIMIT FOR TRAILING END ATTACHMENT INCLUDES ALL HARDWARE AND RAIL ELEMENTS TO AND INCLUDING THE FIRST POST.
5. ALL RUNS OF MERRITT PARKWAY MEDIAN BARRIER SHALL INCLUDE MERRITT PARKWAY MEDIAN BARRIER TRANSITIONS SECTIONS AT BOTH ENDS.
6. THE END OF TIMBER RAIL ATTACHED TO THE ANGLED ATTACHMENT BRACKET FOR THE RIPPWAM RIVER SHALL BE MITERED TO MATCH THE ANGLE OF THE BRACKET AND PARAPET.



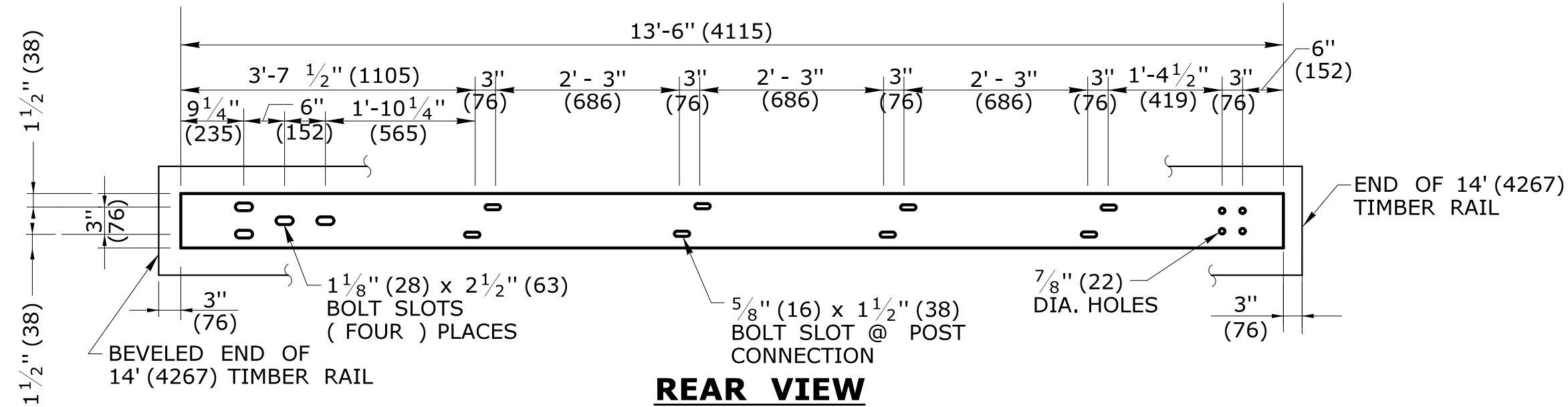
ANGLED ATTACHMENT BRACKET FOR TRAILING END ATTACHMENT AT BRIDGE NO. 0705 (RIPPWAM RIVER BRIDGE)



ATTACHMENT BRACKET

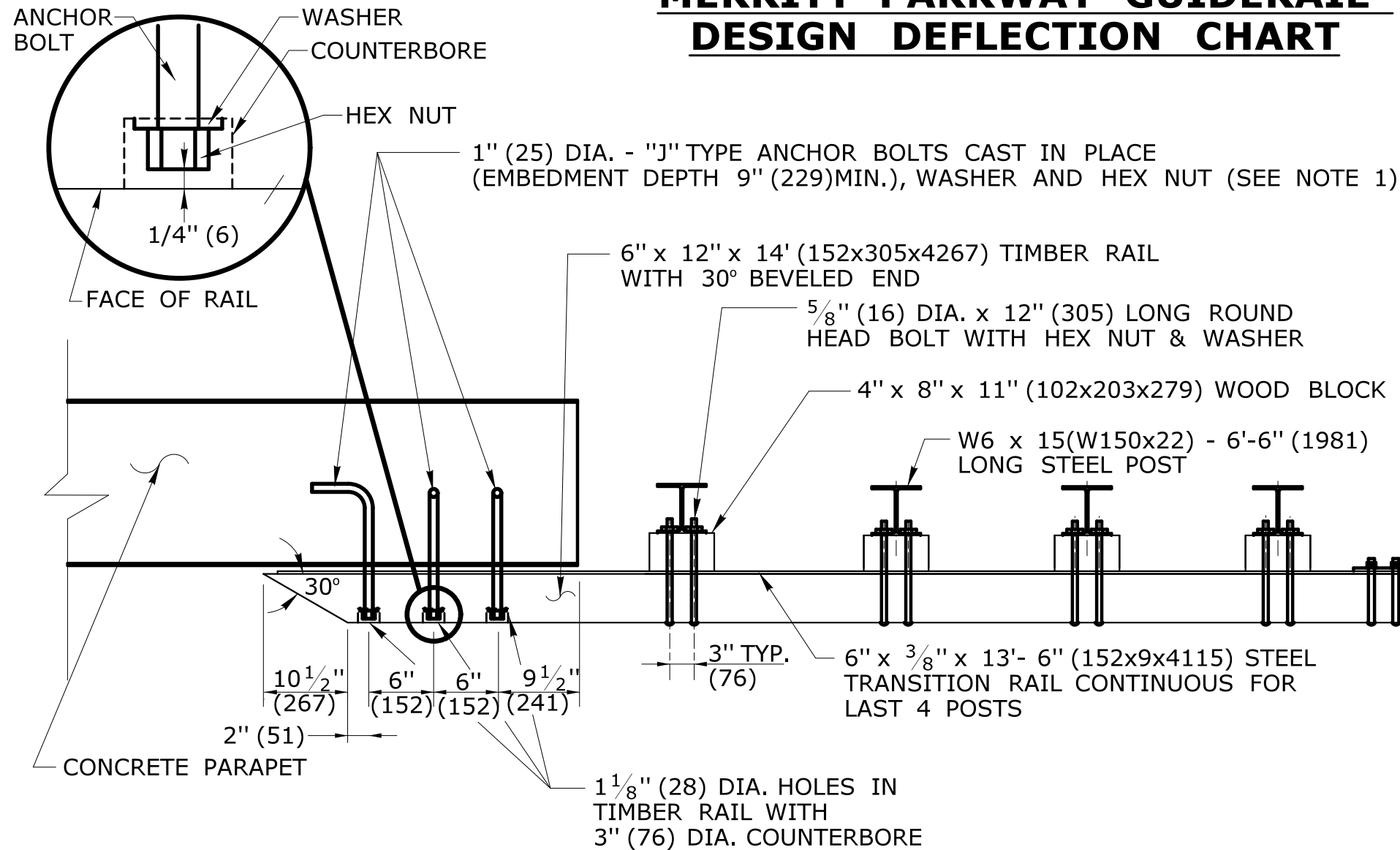
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STEEL TRANSITION RAIL
6" x 3/8" x 13'-6" (152x9x4115)
FOR LEADING END ATTACHMENT

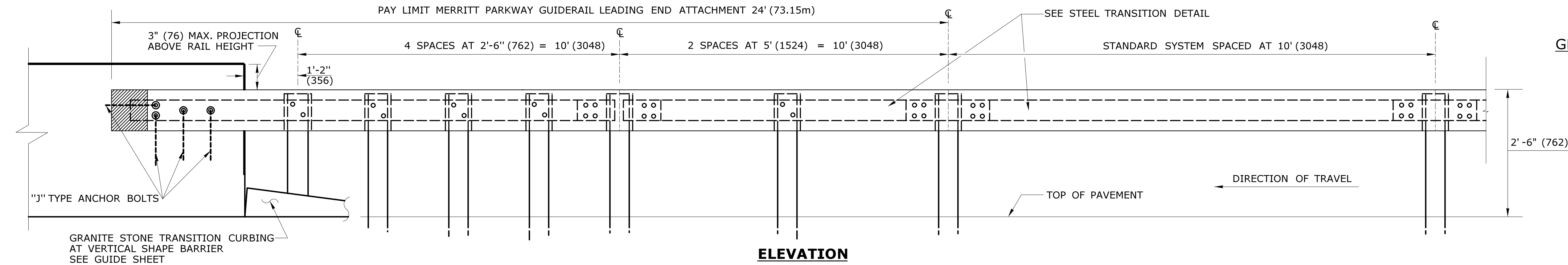


Maximum Design Deflection (ft) (mm)	Area Of Concern Plus 2 Posts (see sketch)	SYSTEM 3	SYSTEM 2	Standard System	L
		W6x15 (W150x22) Posts Spaced at 2'- 6" (762)	W6x15 (W150x22) Posts Spaced at 5' (1524)	W6x15 (W150x22) Posts Spaced at 10' (3048)	
1' (305)	System 3	10' (3048)	10' (3048)	50' (16.40m)	70' (21.34m)
2' - 6" (762)	System 2	—	10' (3048)	60' (19.68m)	70' (21.34m)
4' (1219)	Standard System	—	—	70' (21.34m)	70' (21.34m)

MERRITT PARKWAY GUIDERAIL
DESIGN DEFLECTION CHART



PLAN

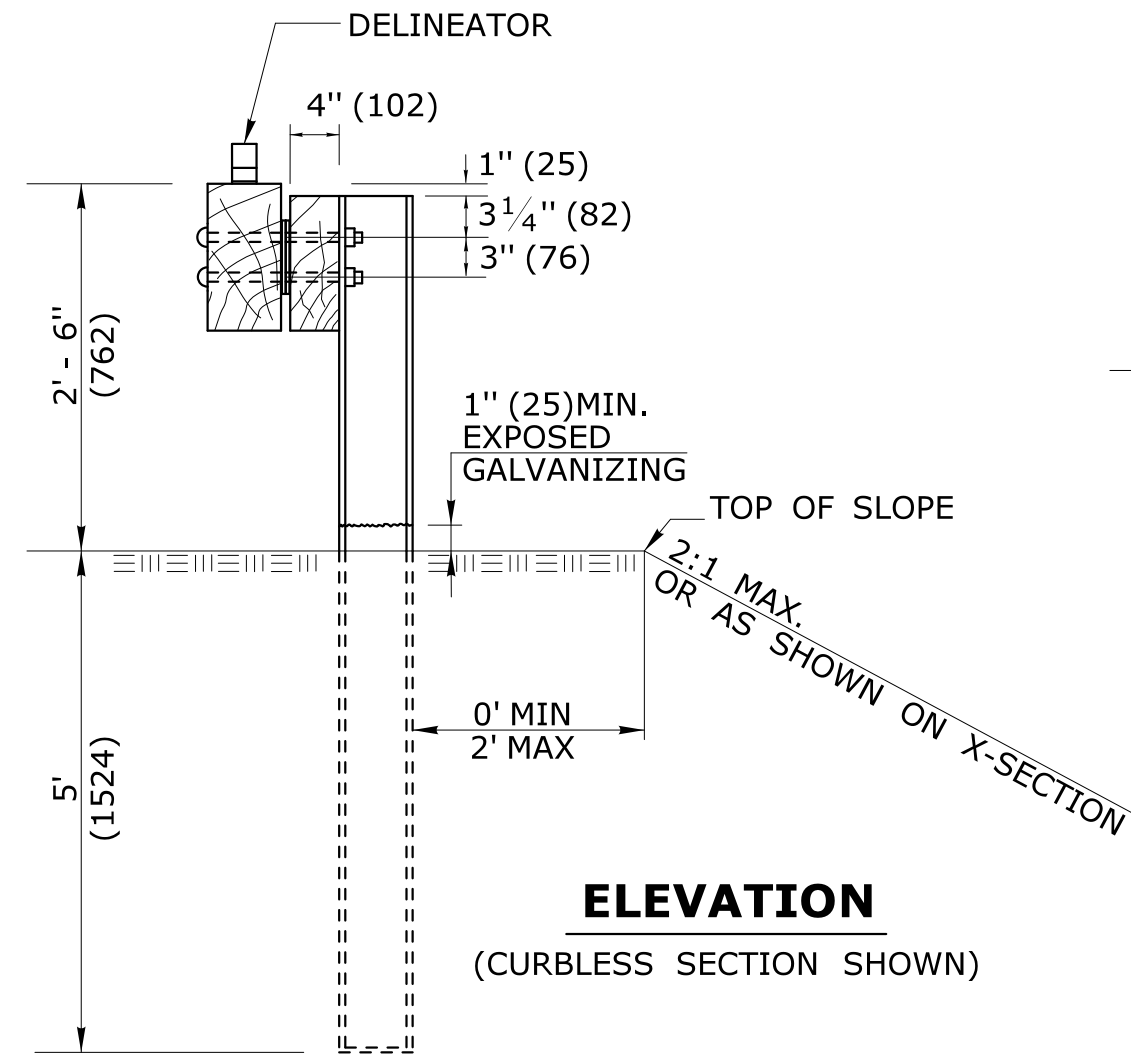


MERRITT PARKWAY GUIDERAIL SYSTEM 2
WITH 7'-6" POSTS

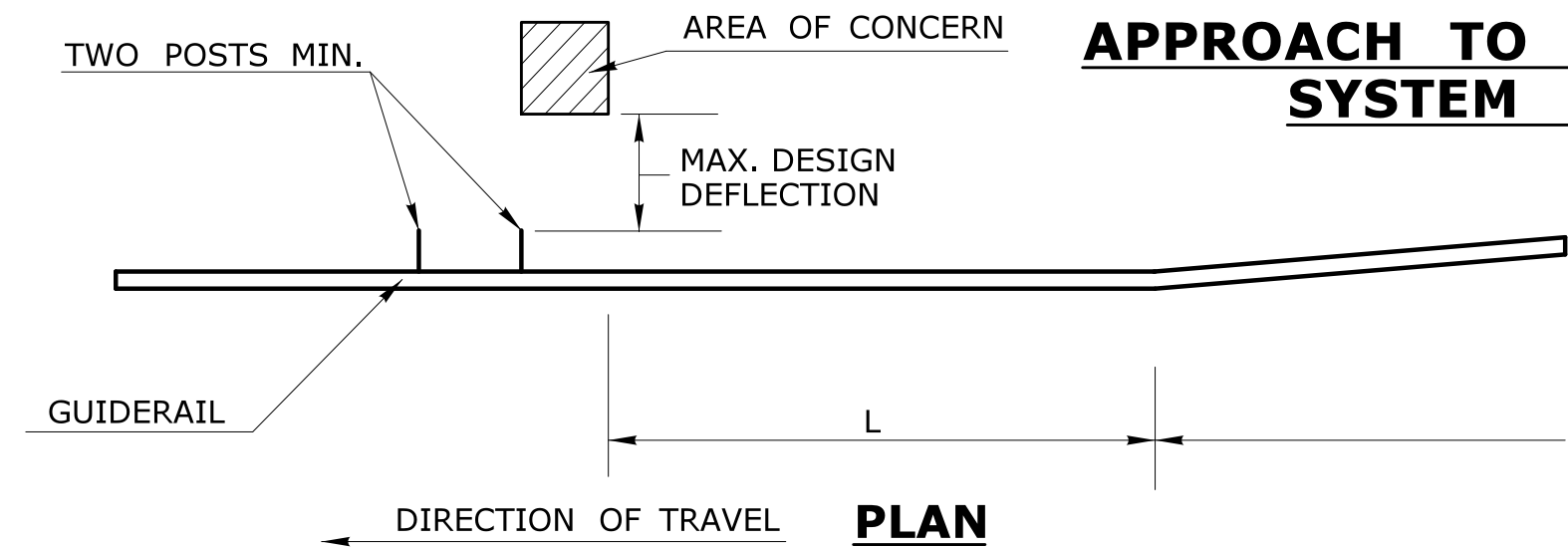
NOTES:

- MERRITT PARKWAY GUIDERAIL SYSTEM 2 WITH 7'-6" POSTS WILL BE PAID FOR UNDER THE CONTRACT ITEM: "MERRITT PARKWAY GUIDERAIL SYSTEM 2"

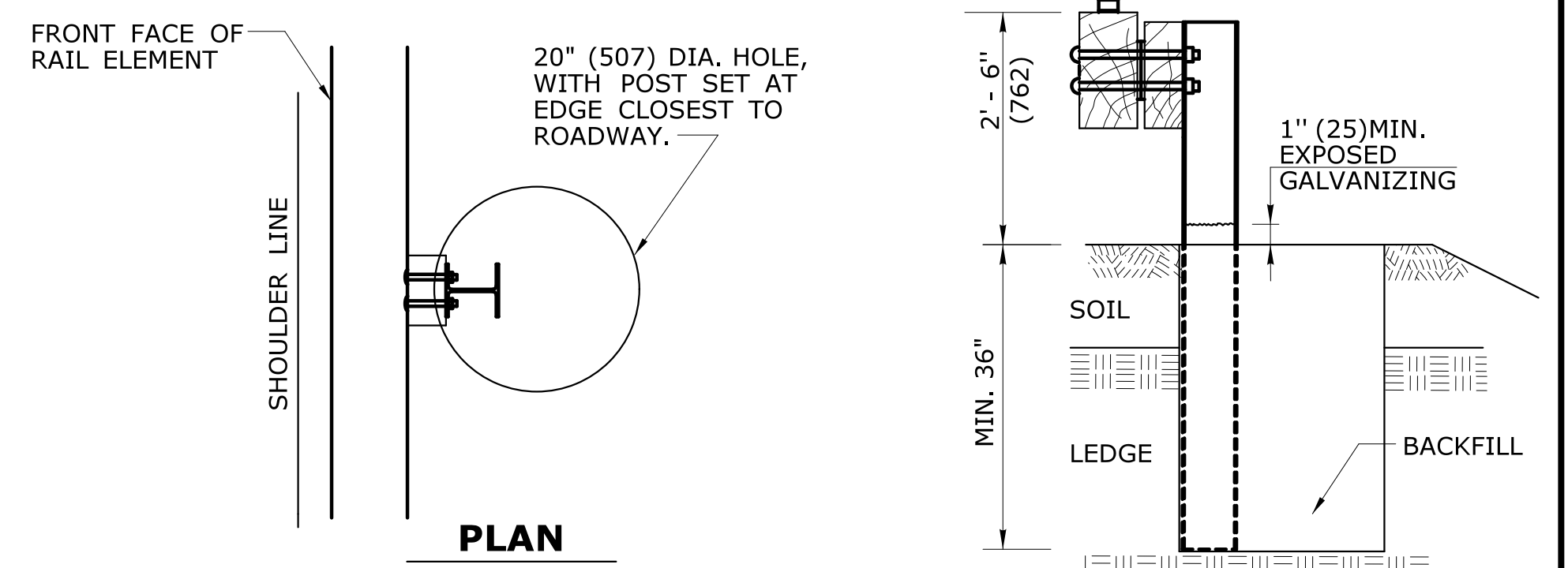
ELEVATION
(CURBLESS SECTION SHOWN)



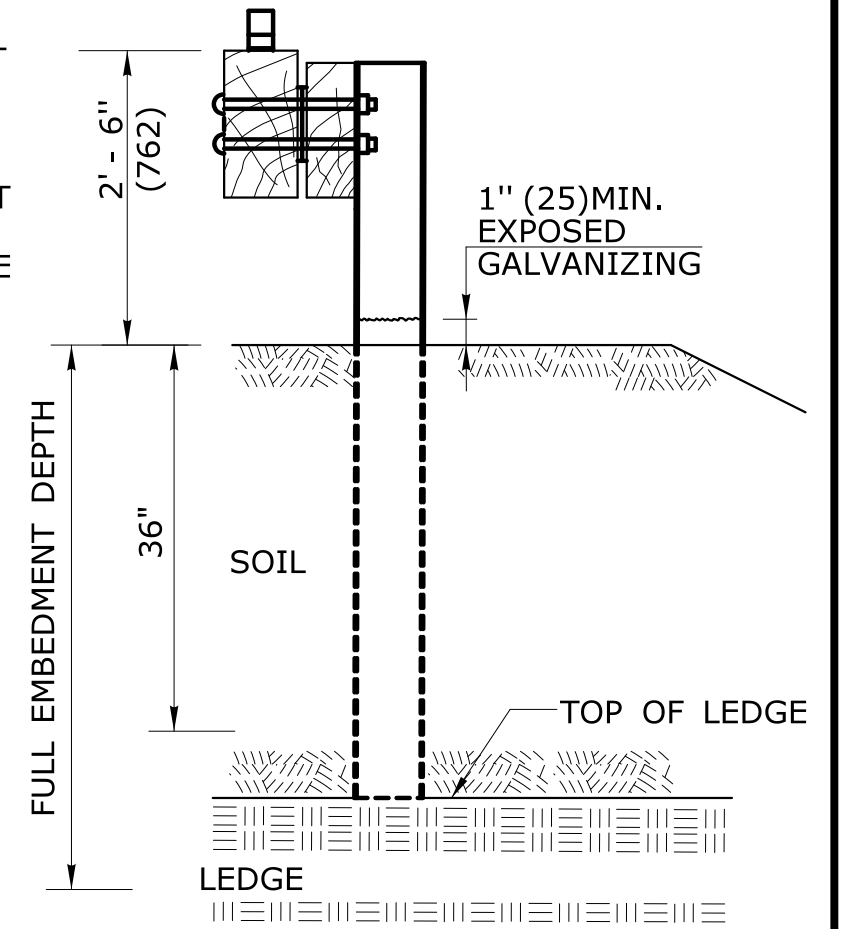
APPROACH TO FIXED OBJECTS
SYSTEM 2 AND 3



DRILLING HOLE FOR GUIDERAIL POST
(SEE GENERAL NOTES 2-3)



CASE 1
(CURBLESS SECTION SHOWN)





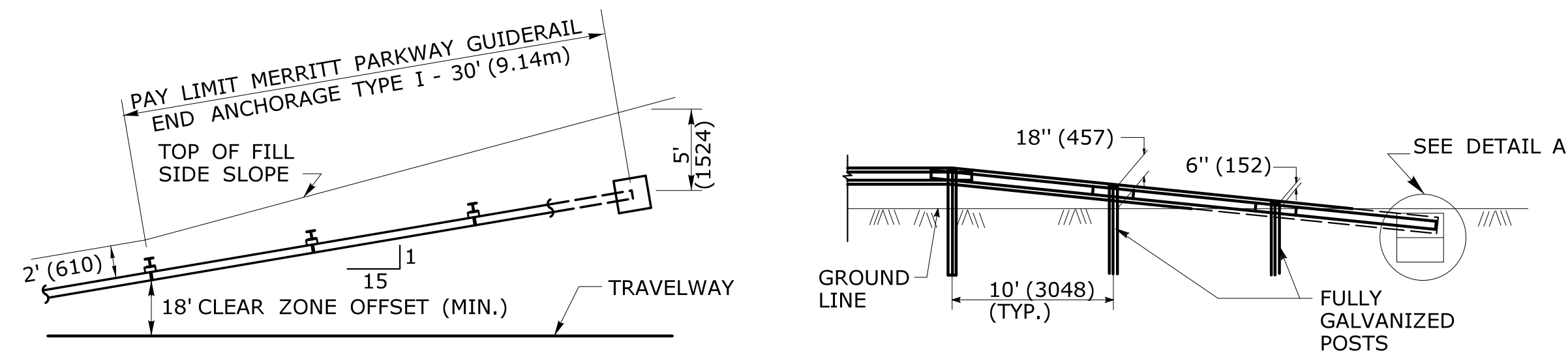
CASE 2
(CURBLESS SECTION SHOWN)

- CASE 1: IF LEDGE IS LOCATED WITHIN 36" OF THE FINISHED GRADE AT THE POST LOCATION A 20" DIAMETER HOLE MUST BE DRILLED IN THE LEDGE TO THE MINIMUM EMBEDMENT DEPTH OF 36" AND THE POST MUST BE CUT AND DRIVEN TO ACHIEVE THIS DEPTH.
- CASE 2: IF LEDGE IS LOCATED AT A DEPTH GREATER THAN 36" BUT LESS THAN THE FULL EMBEDMENT DEPTH THE POST SHALL BE CUT AND DRIVEN TO ACHIEVE EMBEDMENT TO THE DEPTH OF THE TOP OF LEDGE.

GENERAL NOTES:

- THIS DRAWING SHOWS LEADING END ATTACHMENT TO PROPOSED BARRIER/PARAPET. AT EXISTING PARAPETS ATTACH GUIDERAIL USING D.O.T. APPROVED CHEMICAL ANCHORS.
- PRIOR TO GUIDERAIL POST INSTALLATION THE CONTRACTOR SHALL INVESTIGATE POST LOCATIONS FOR THE PRESENCE AND DEPTH OF LEDGE.
- 20" (507) DIA. EXCAVATED HOLE SHALL BE BACKFILLED WITH SUITABLE MATERIAL, OR GRANULAR FILL COMPACTED IN 6" (150) LIFTS BEFORE DRIVING POST OR POSTS MAY BE SET IN EXCAVATED HOLE AND BACKFILLED WITH CONTROLLED LOW STRENGTH MATERIAL (CLSM).

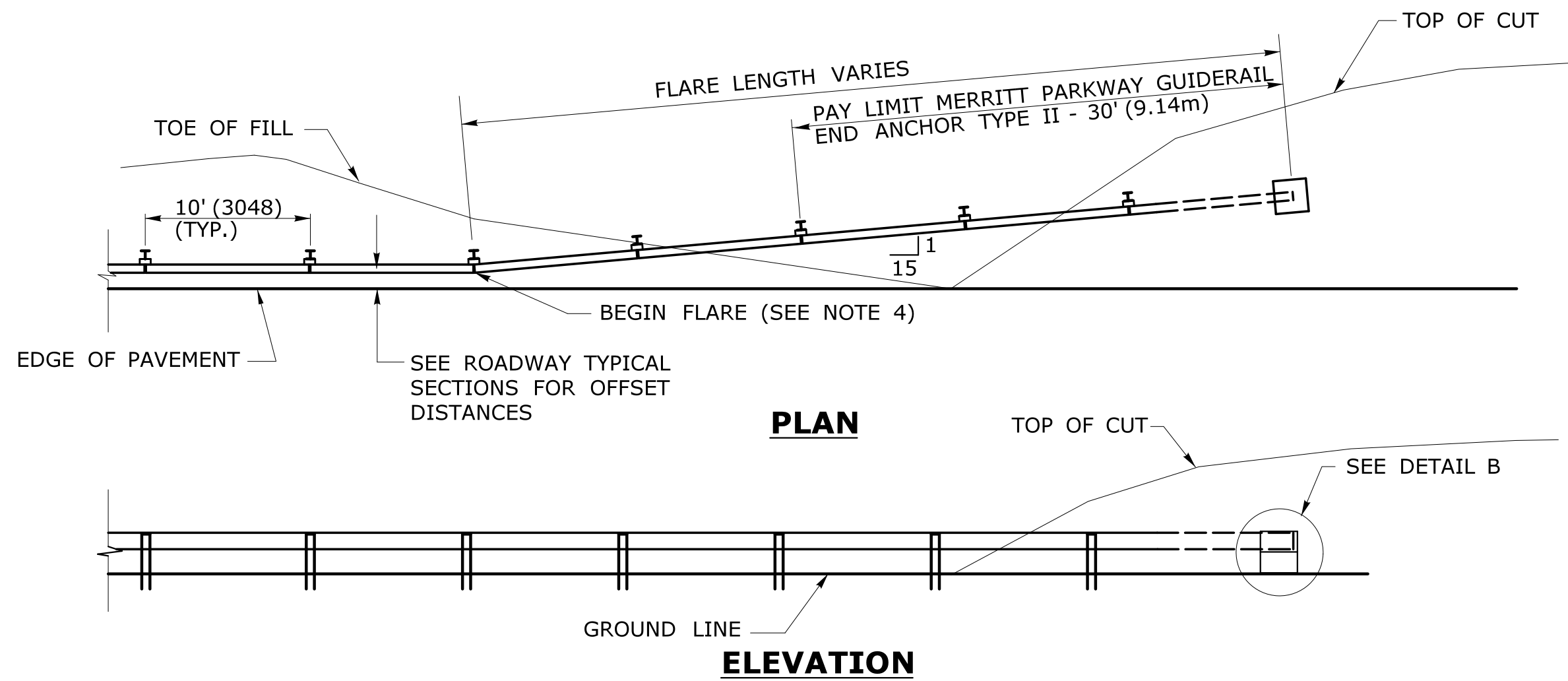
				THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		DESIGNER/DRAFTER: - CHECKED BY: - SCALE AS NOTED		 STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION Filename: ...\\158-211_Guide-03_MPGR_Leading_Attach...dgn		SIGNATURE/ BLOCK: APPROVED BY: 		PROJECT TITLE: ROUTE 15 SAFETY IMPROVEMENTS, RESURFACING, ENHANCEMENTS, AND BRIDGE IMPROVEMENTS		TOWN: WESTPORT FAIRFIELD		PROJECT NO. 158-211	
														DRAWING TITLE: MERRITT PARKWAY GUIDERAIL		DRAWING NO. GDS-03	
																SHEET NO. 01.03.10	
REV.	DATE	REVISION	DESCRIPTION	SHEET NO.	Plotted Date: 7/18/2016												



PLAN

ELEVATION

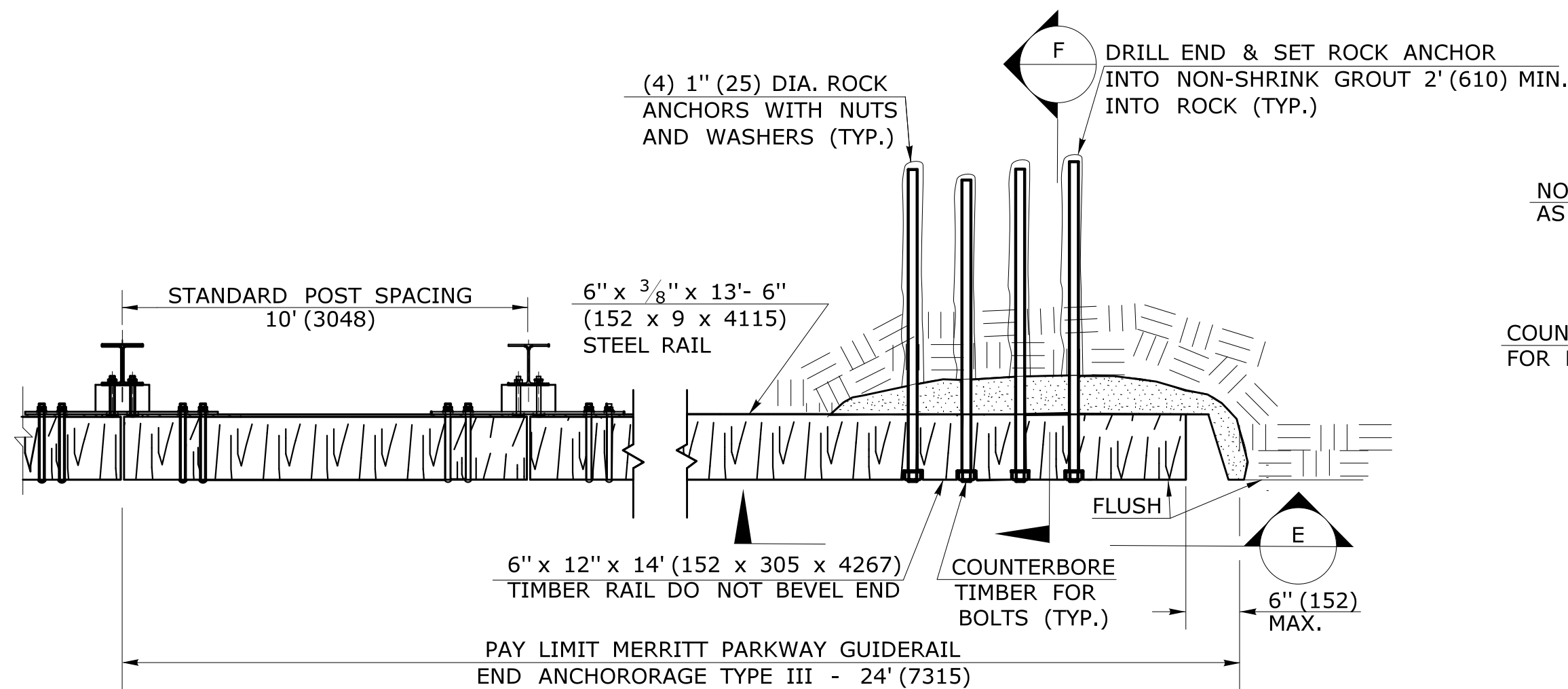
**MERRITT PARKWAY GUIDERAIL END ANCHORAGE TYPE I
(BURIED ANCHOR)**



PLAN

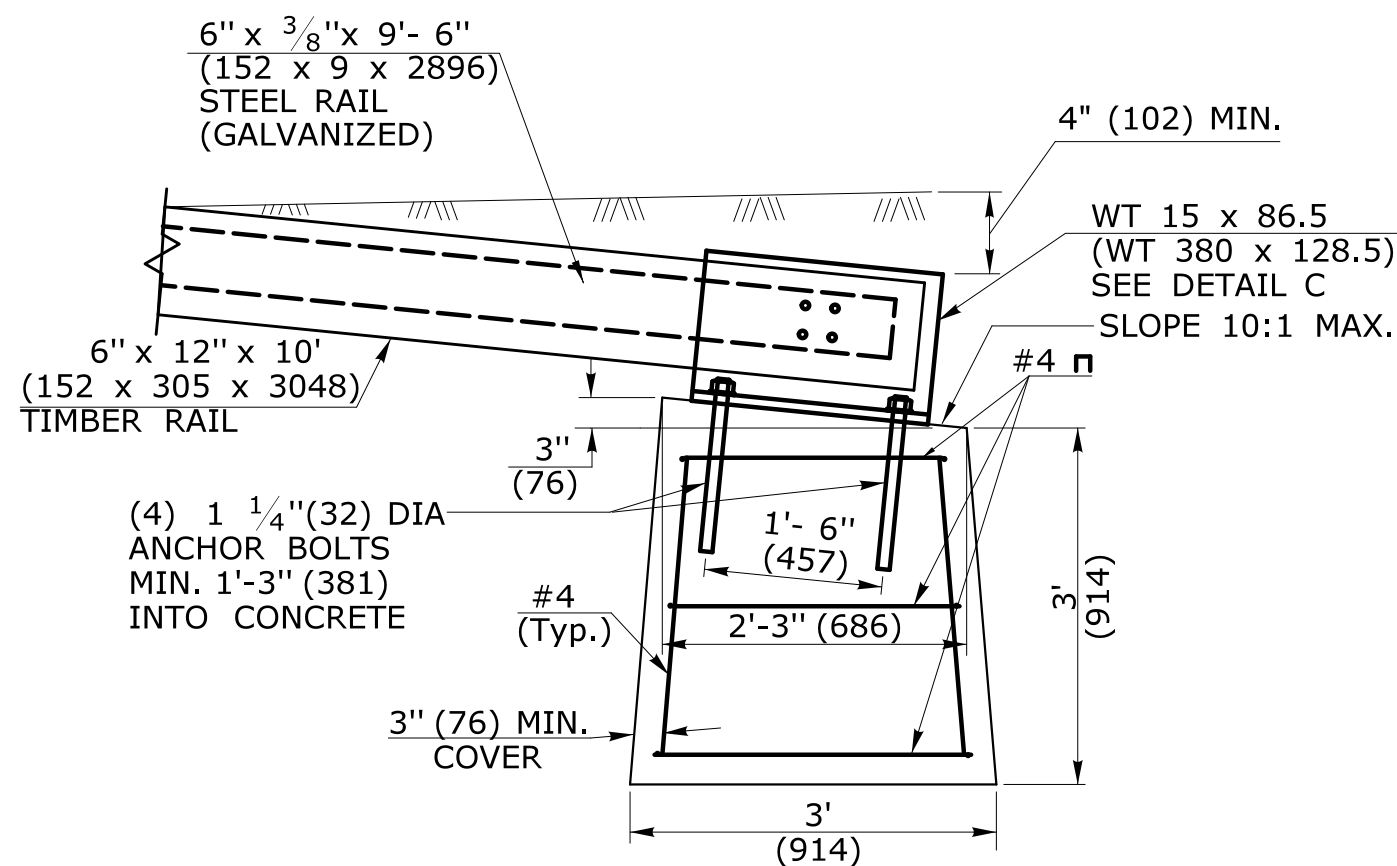
ELEVATION

**MERRITT PARKWAY GUIDERAIL END ANCHORAGE TYPE II
(EARTH CUT SLOPE ANCHOR)**

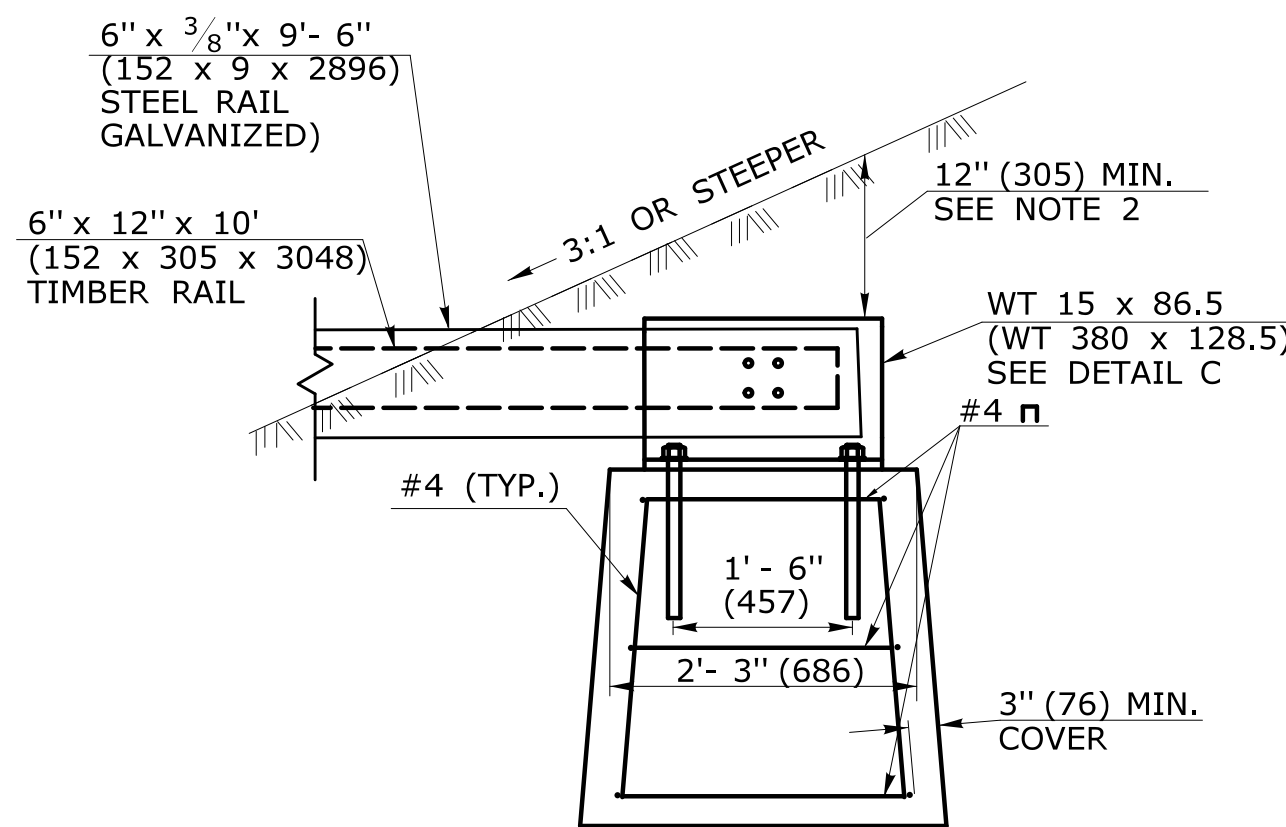


PLAN

**MERRITT PARKWAY GUIDERAIL END ANCHORAGE TYPE III
(ROCK CUT ANCHOR)**



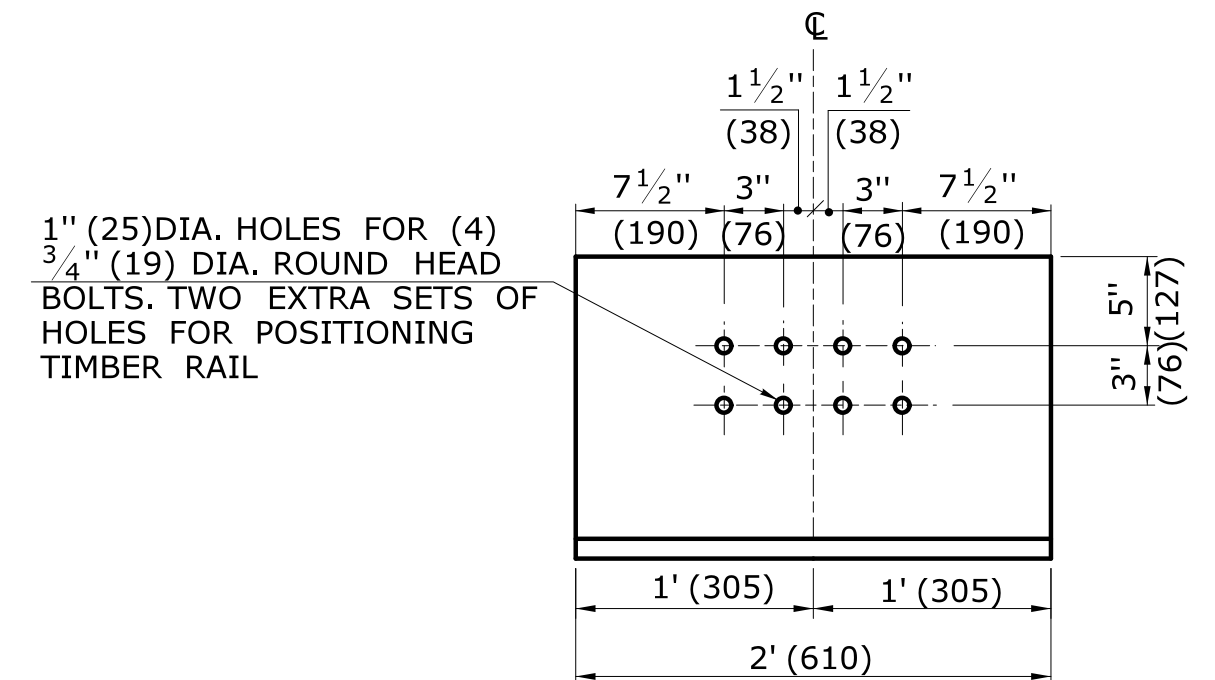
ELEVATION
DETAIL A



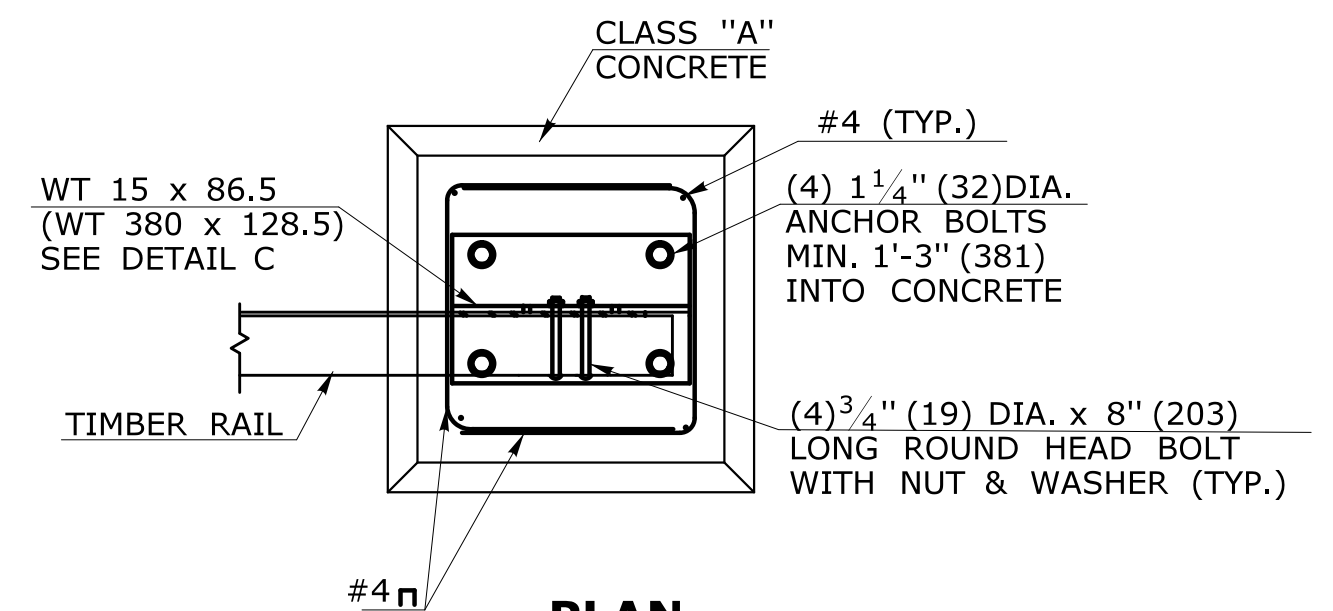
ELEVATION
DETAIL B

GENERAL NOTES:

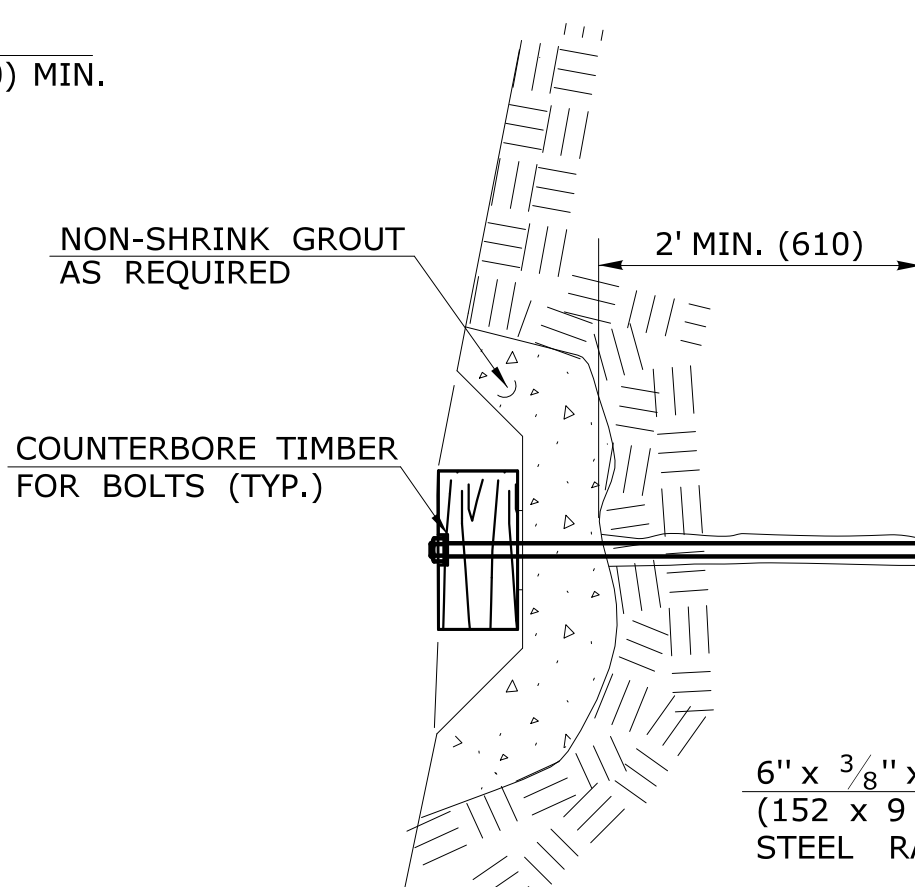
1. MATERIALS AND CONSTRUCTION OF THE MERRITT PARKWAY GUIDERAIL END ANCHORS SHALL CONFORM TO THE SPECIAL PROVISIONS PROVIDED WITH THE PROJECT. ALL HARDWARE IN CONTACT WITH THE GROUND SHALL BE GALVANIZED IN ACCORDANCE WITH THE SPECIAL PROVISION.
2. FOR THE END ANCHOR TYPE II, EXTEND THE FLARE INTO THE CUT SLOPE UNTIL A MINIMUM 12" (305) OF COVER IS OBTAINED OVER THE GUIDERAIL ELEMENT.
3. TYPE I END ANCHORS SHALL BE INSTALLED OUTSIDE THE CLEAR ZONE. THE CLEAR ZONE FOR THE MERRITT PARKWAY IS 18' AND IS MEASURED FROM THE EDGE OF TRAVEL WAY.
4. BEGIN THE FLARE AT THE NEAREST POST TO A TRANSITION POINT BETWEEN FILL AND CUT AS DIRECTED BY THE ENGINEER.



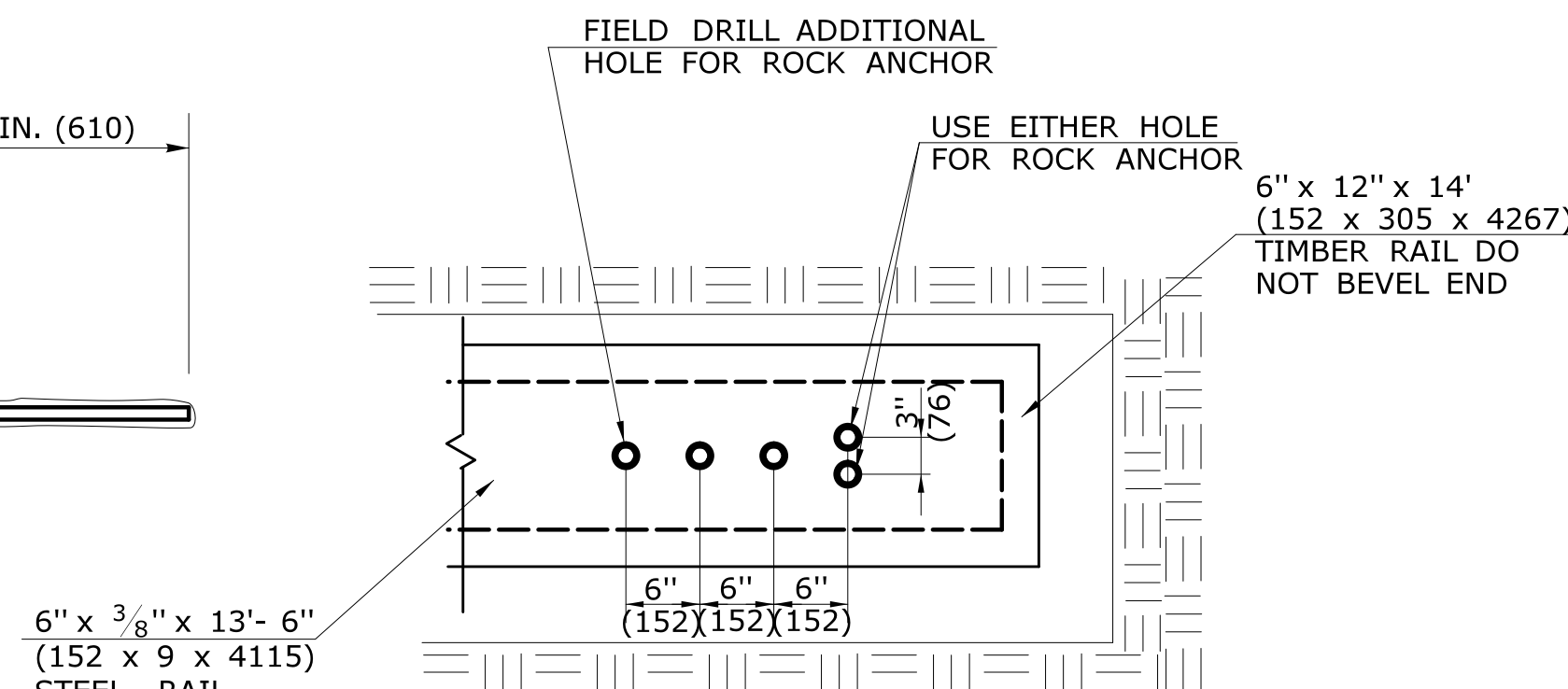
ELEVATION
DETAIL C
**WT 15 x 86.5 (GALVANIZED)
(WT 380 X 128.5)**



PLAN
DETAIL D
**PLAN VIEW FOR
TYPE I & II ANCHOR**



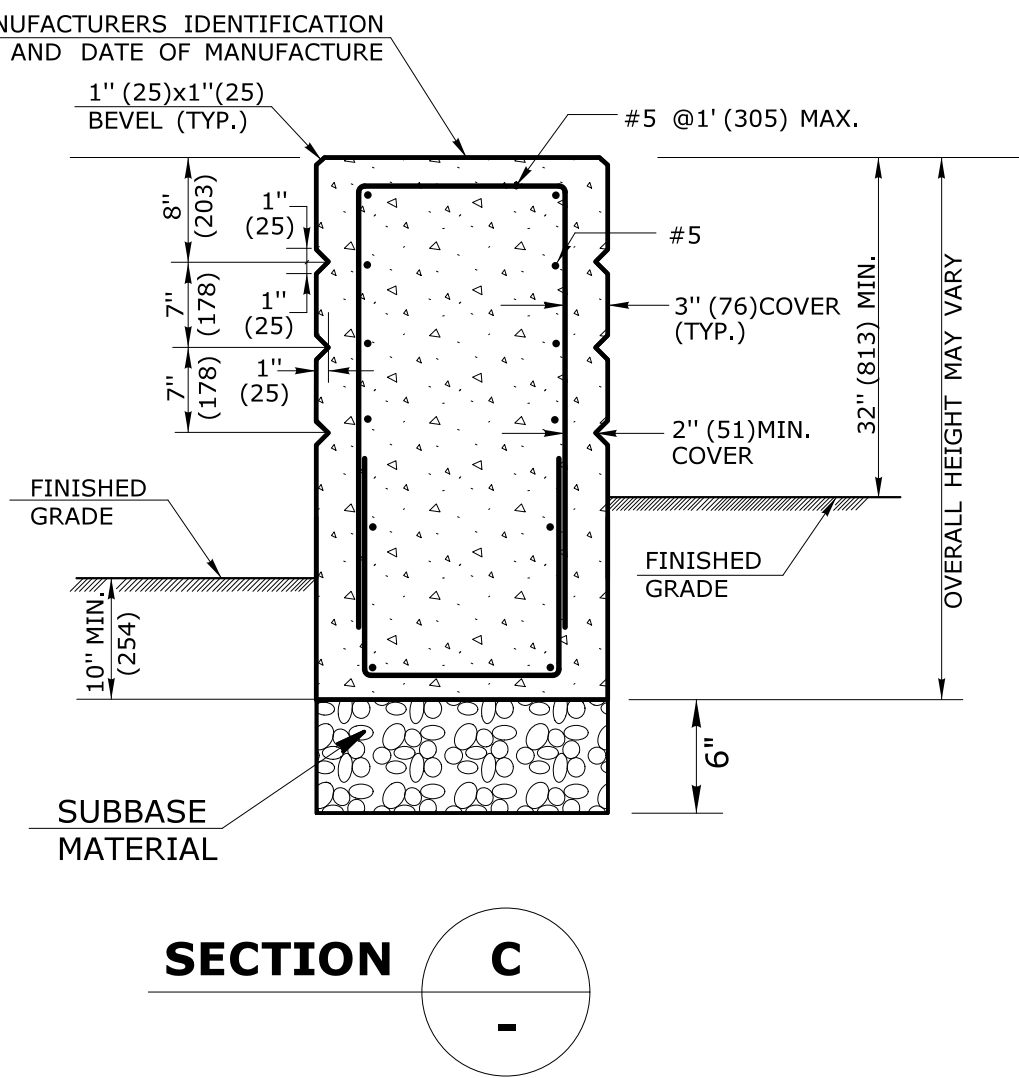
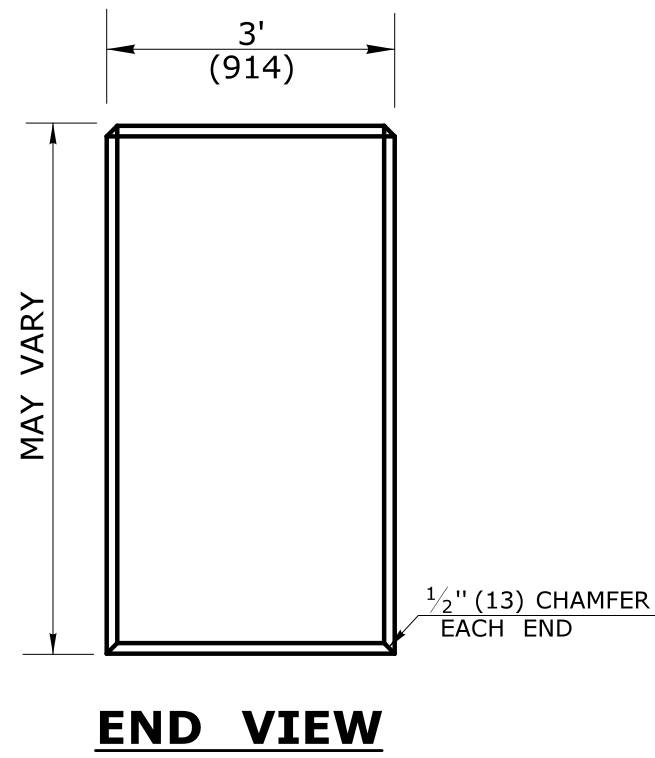
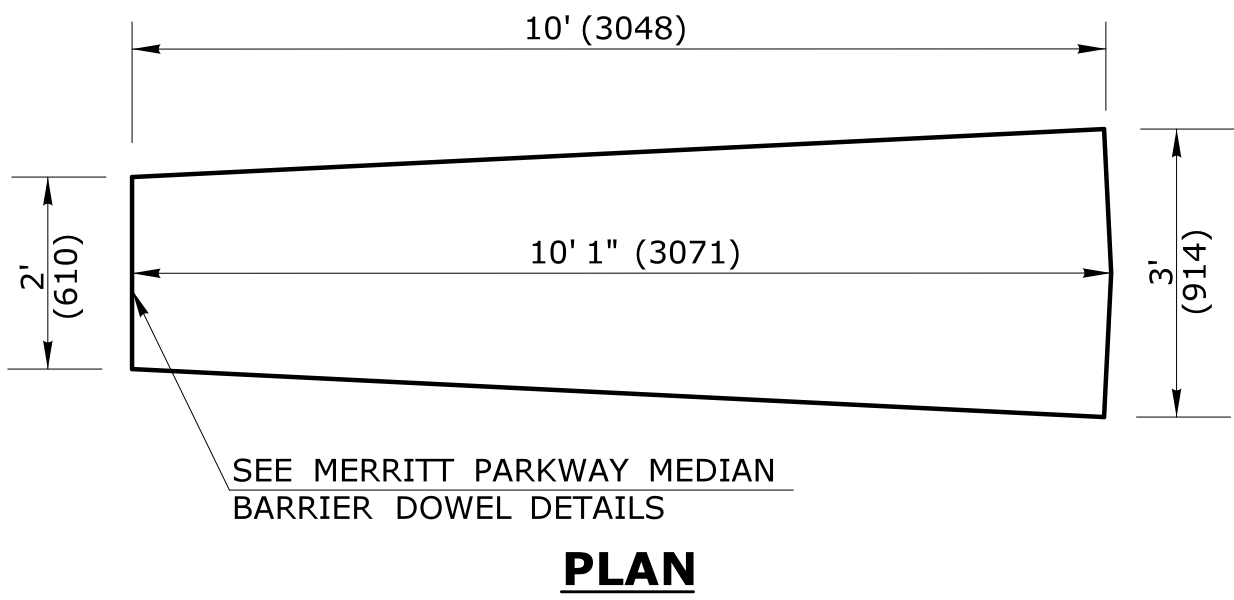
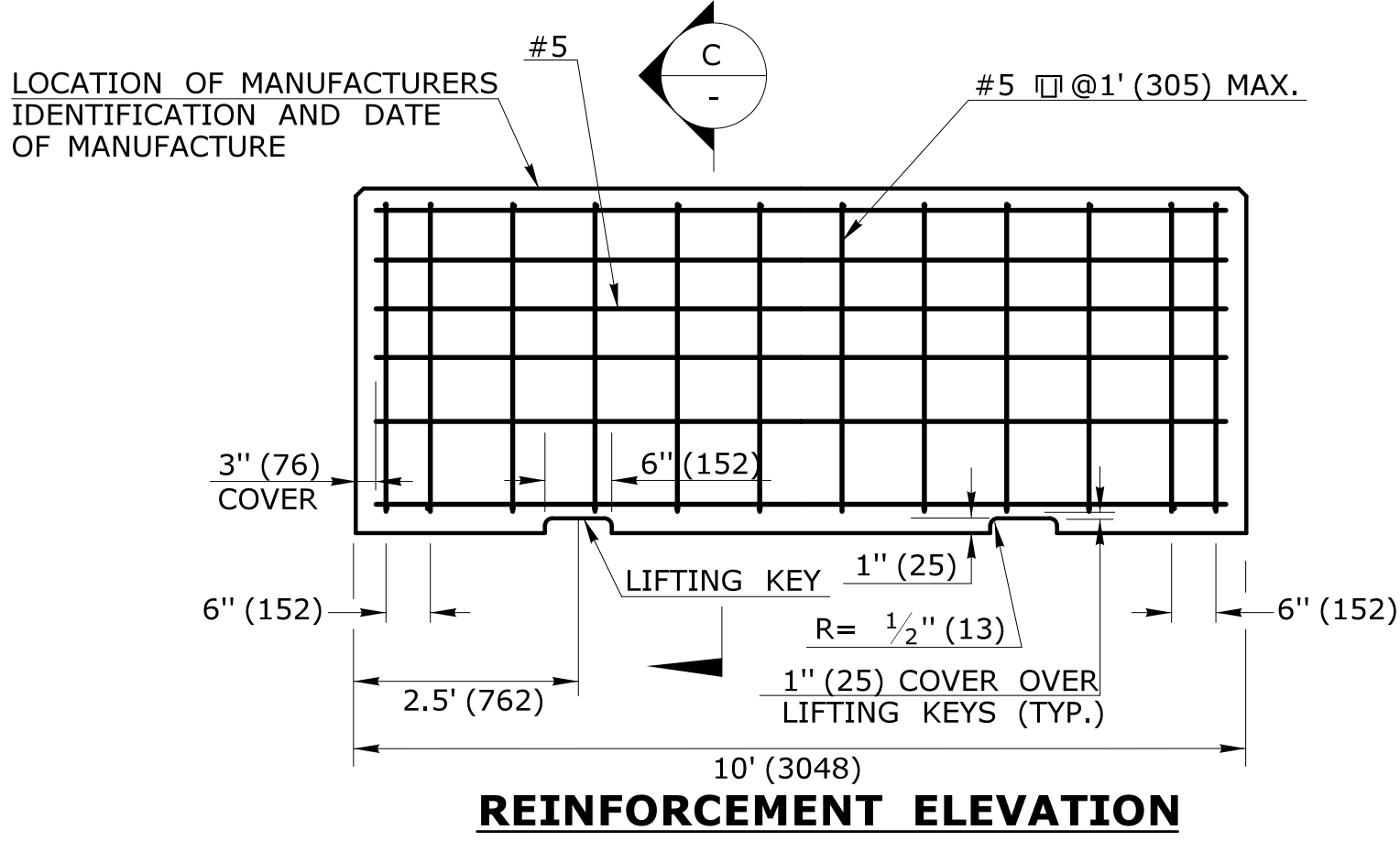
SECTION F



SECTION E

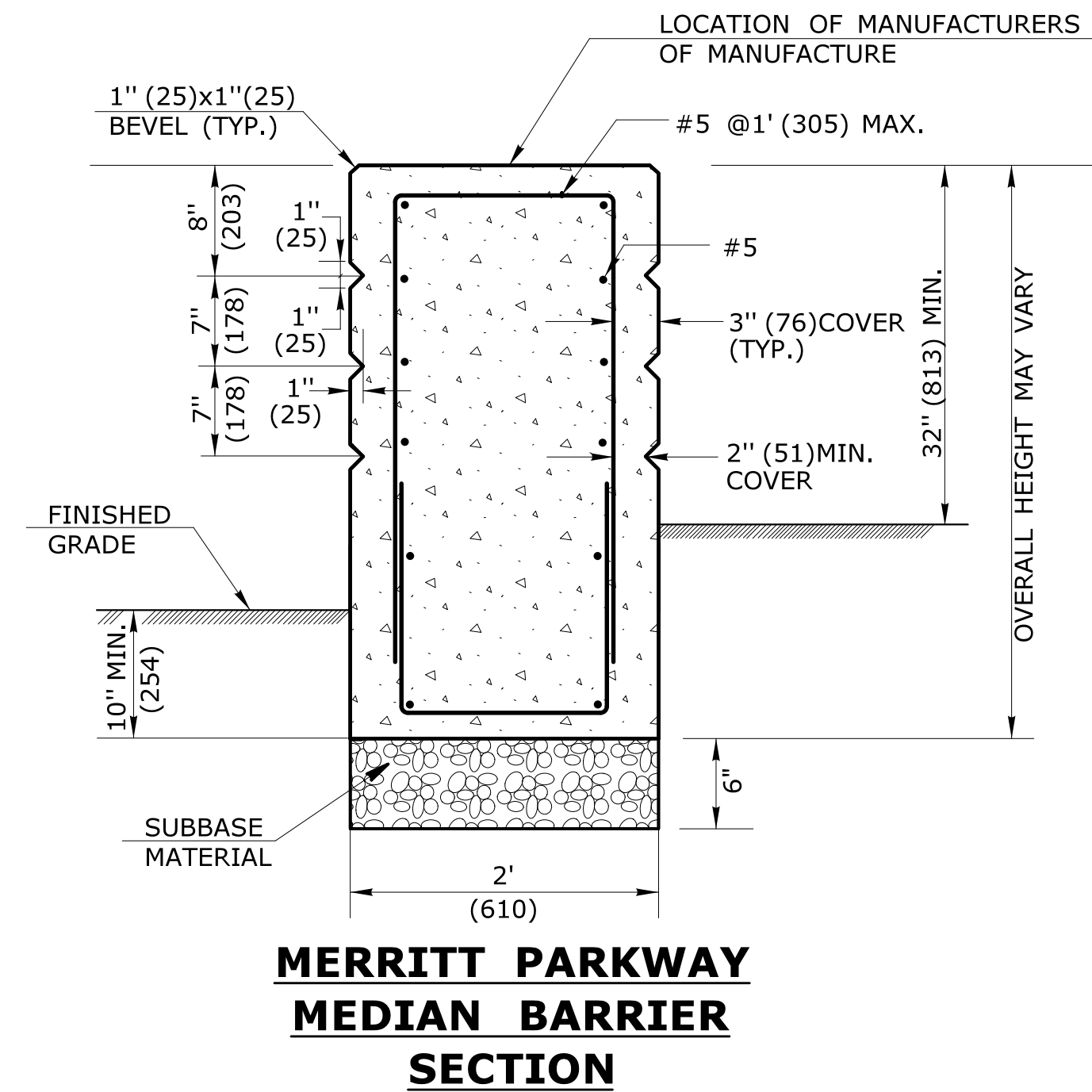
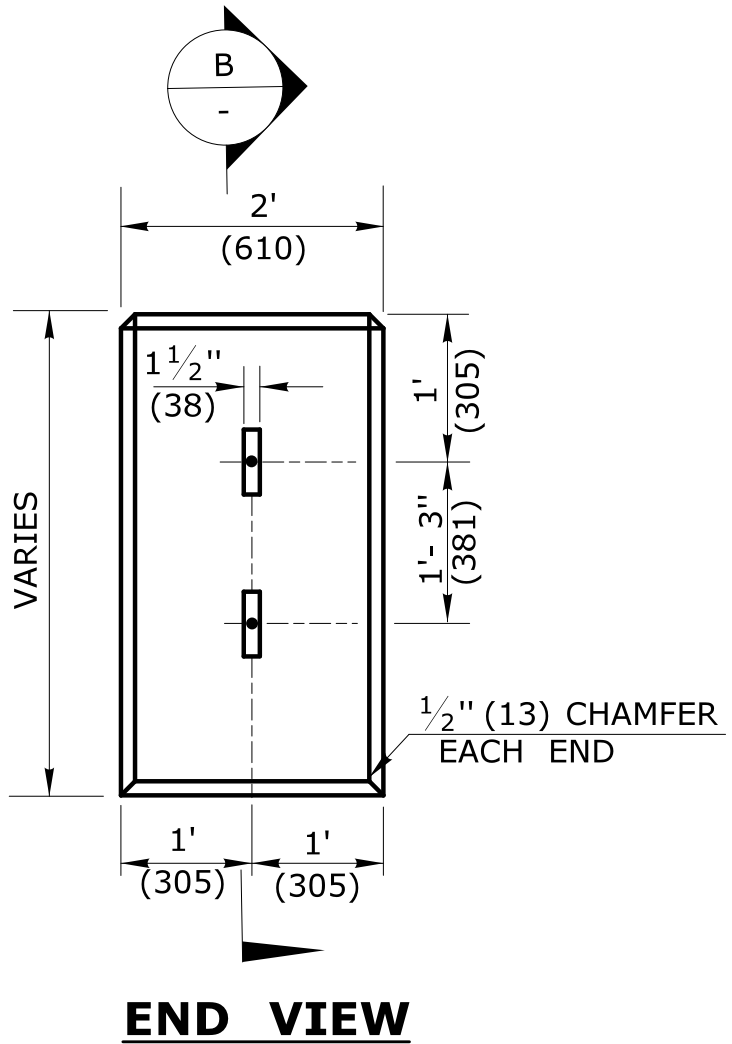
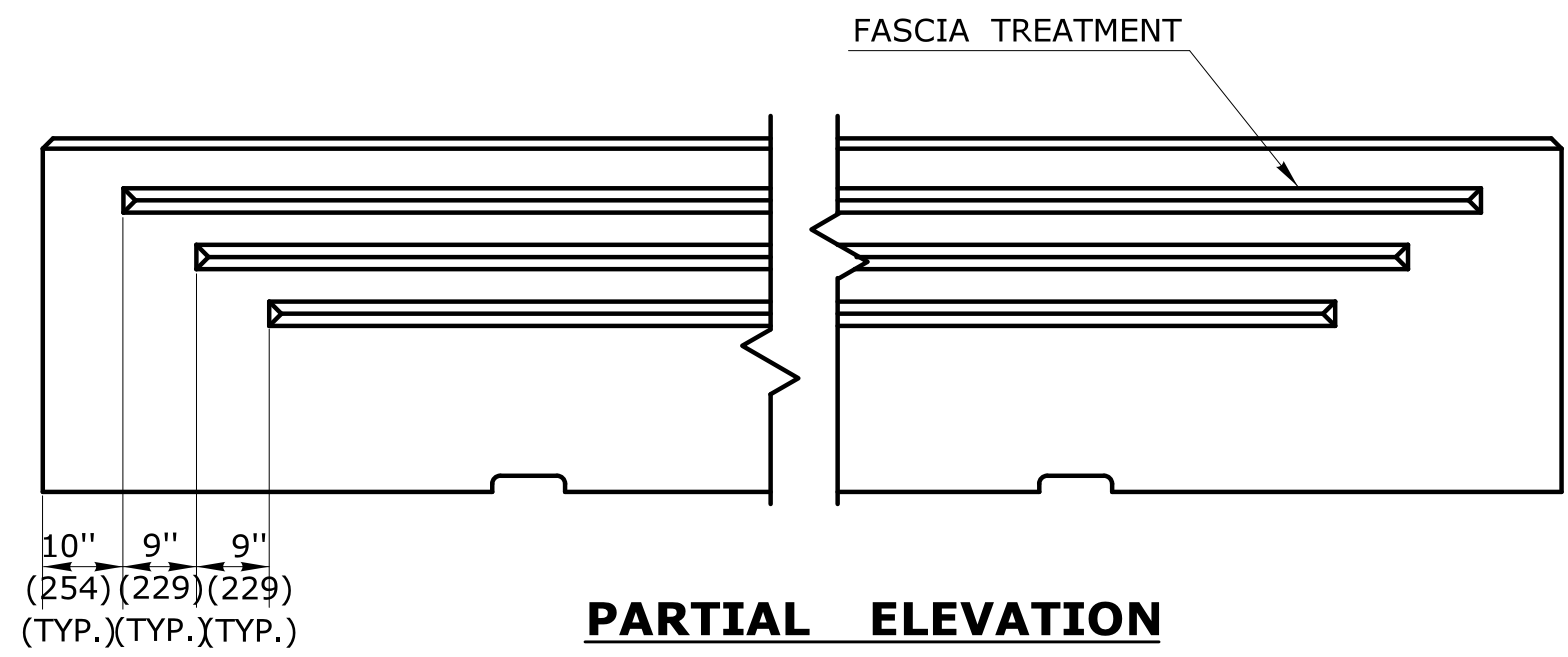
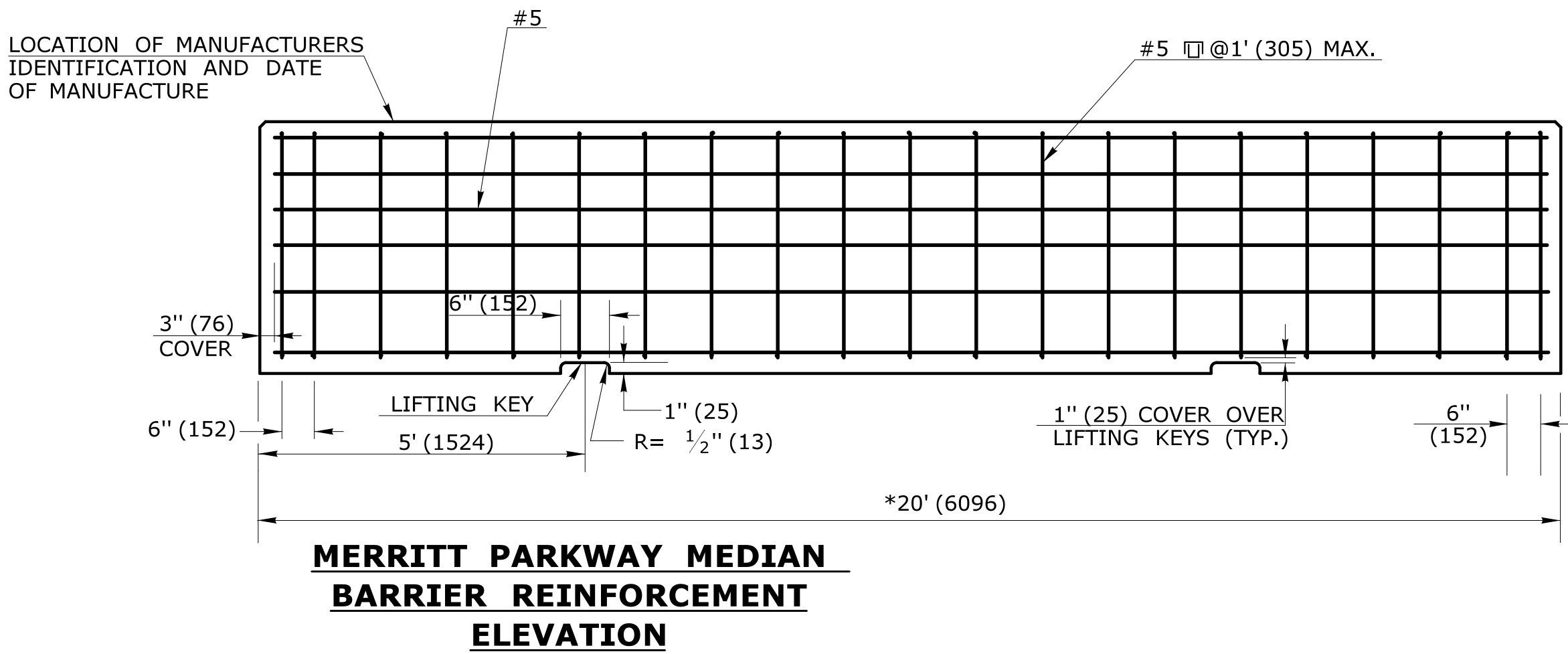
ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 7/18/2016	DESIGNER/DRAFTER: CHECKED BY: SCALE AS NOTED	THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.	STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION Filename: ...\\158-211_Guide_04_MPGR_End_Anchor.dgn	SIGNATURE/ BLOCK: OFFICE OF ENGINEERING APPROVED BY: <i>Willie B. Hall</i>	PROJECT TITLE: ROUTE 15 SAFETY IMPROVEMENTS, RESURFACING, ENHANCEMENTS, AND BRIDGE IMPROVEMENTS	TOWN: WESTPORT FAIRFIELD DRAWING TITLE: MERRITT PARKWAY GUIDERAIL	PROJECT NO. 158-211 DRAWING NO. GDS-04 SHEET NO. 01.03.11
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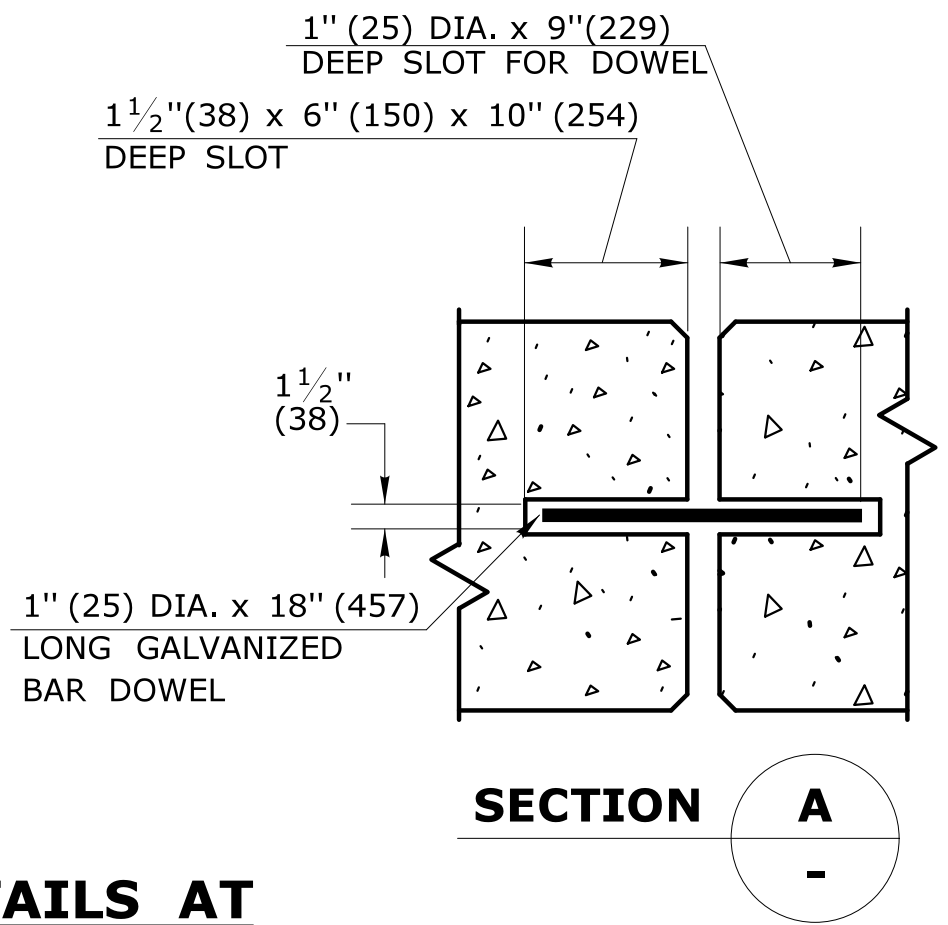
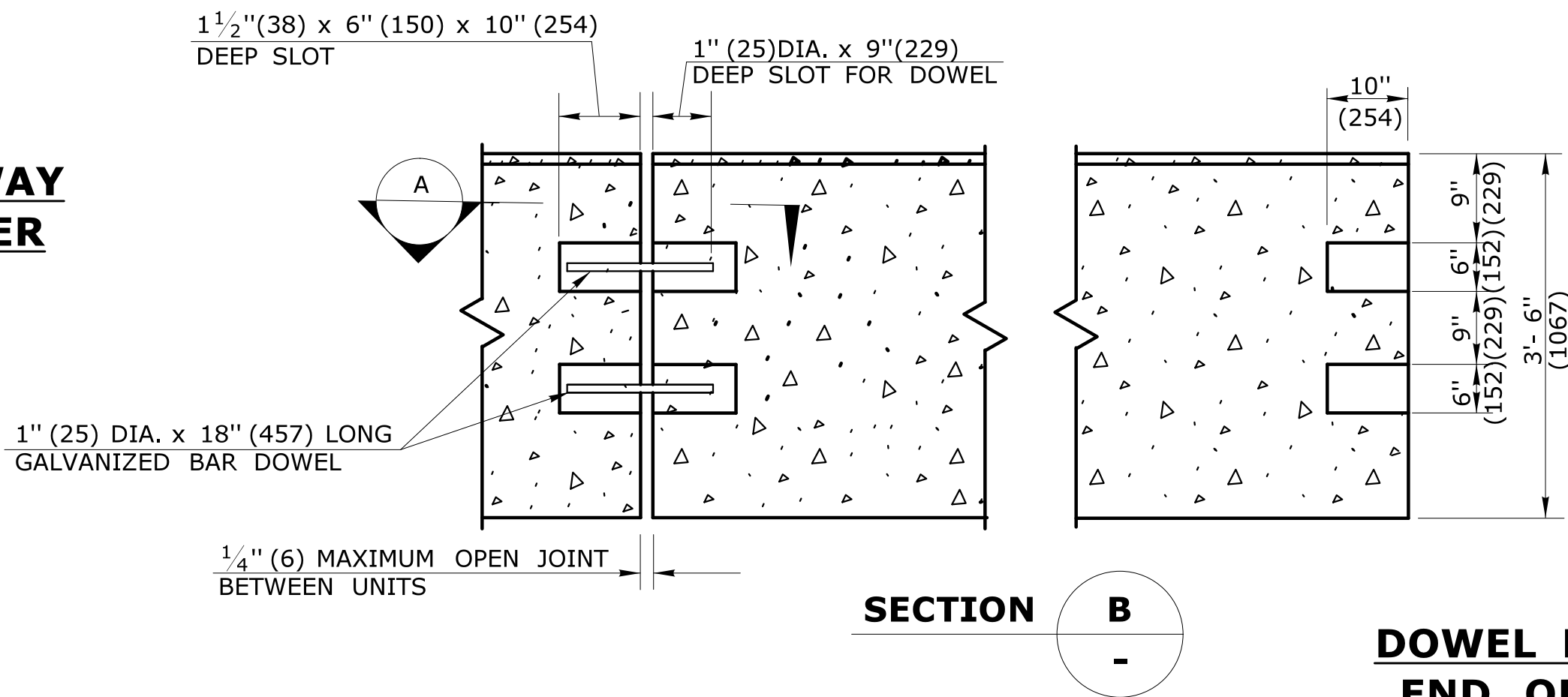


GENERAL NOTES:

1. ALTERNATE DESIGN FOR THE FOLLOWING MAY BE SUBMITTED TO THE ENGINEER FOR APPROVAL:
 - a. UNIT END CONNECTIONS SIMILAR TO THE DESIGN SHOWN.
 - b. LIFTING HOLES, KEYS OR OTHER HANDLING DEVICES.
 2. PRECAST UNITS SHALL BE COATED WITH A PENETRATING SEALER.
 3. REINFORCING SHALL BE UNCOATED AND CONFORM TO ASTM A615, GRADE 60.(420)
 4. CONCRETE SHALL HAVE A MINIMUM 28 DAY STRENGTH (f'_c) = 4,000 PSI. (28 MPa)
 5. DOWELS SHALL CONFORM TO ASTM A36 AND BE GALVANIZED IN ACCORDANCE WITH ASTM A123.
 6. ALL RUNS OF MERRITT PARKWAY MEDIAN BARRIER SHALL INCLUDE A MERRITT PARKWAY MEDIAN TRANSITION PIECE AT BOTH ENDS OF THE RUN TO ENSURE PROPER CONNECTION TO GUIDERAIL. MERRITT PARKWAY MEDIAN BARRIER TRANSITION PIECE WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR "MERRITT PARKWAY MEDIAN BARRIER"
 7. THE 2' WIDE END OF THE MERRITT PARKWAY MEDIAN BARRIER TRANSITION PIECES SHALL BE SLOTTED AND DOWED IN ACCORDANCE WITH THE DETAILS SHOWN. THE 3' WIDE END OF THE PIECE SHALL NOT HAVE SLOTS OR DOWELS.
 7. TERMINAL END TREATMENTS SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
 8. THE BARRIER SHALL BE INSTALLED TO PRODUCE A SMOOTH VERTICAL PROFILE.
- * THE LENGTH OF MERRITT PARKWAY MEDIAN BARRIER PRECAST UNITS CAN BE REDUCED TO A MINIMUM OF 10' WHERE REQUIRED.



MERRITT PARKWAY MEDIAN BARRIER

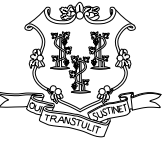


DOWEL DETAILS AT END OF BARRIER

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

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DESIGNER/DRAFTER:	-
CHECKED BY:	-
SCALE AS NOTED	

**STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION**

Filename: ...\\158-211_Guide_05_MPMB_MPRSB.dgn

SIGNATURE/
BLOCK:

OFFICE OF ENGINEERING

APPROVED BY:

Willie B. Smith

PROJECT TITLE:

**ROUTE 15 SAFETY IMPROVEMENTS,
RESURFACING, ENHANCEMENTS,
AND BRIDGE IMPROVEMENTS**

TOWN:

**WESTPORT
FAIRFIELD**

DRAWING TITLE:

**MERRITT PARKWAY
BARRIER**

PROJECT NO.

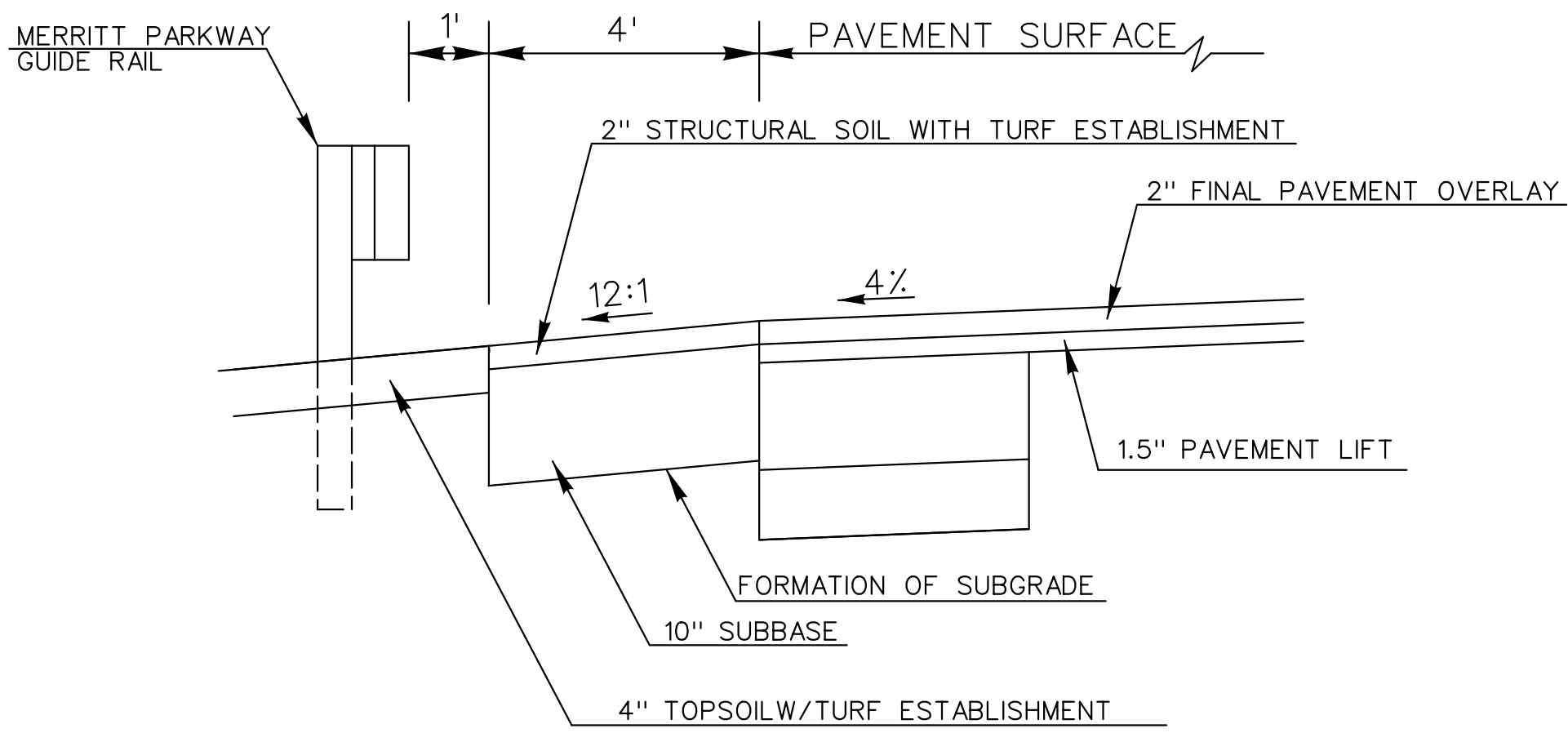
158-211

DRAWING NO.

GDS-05

SHEET NO.

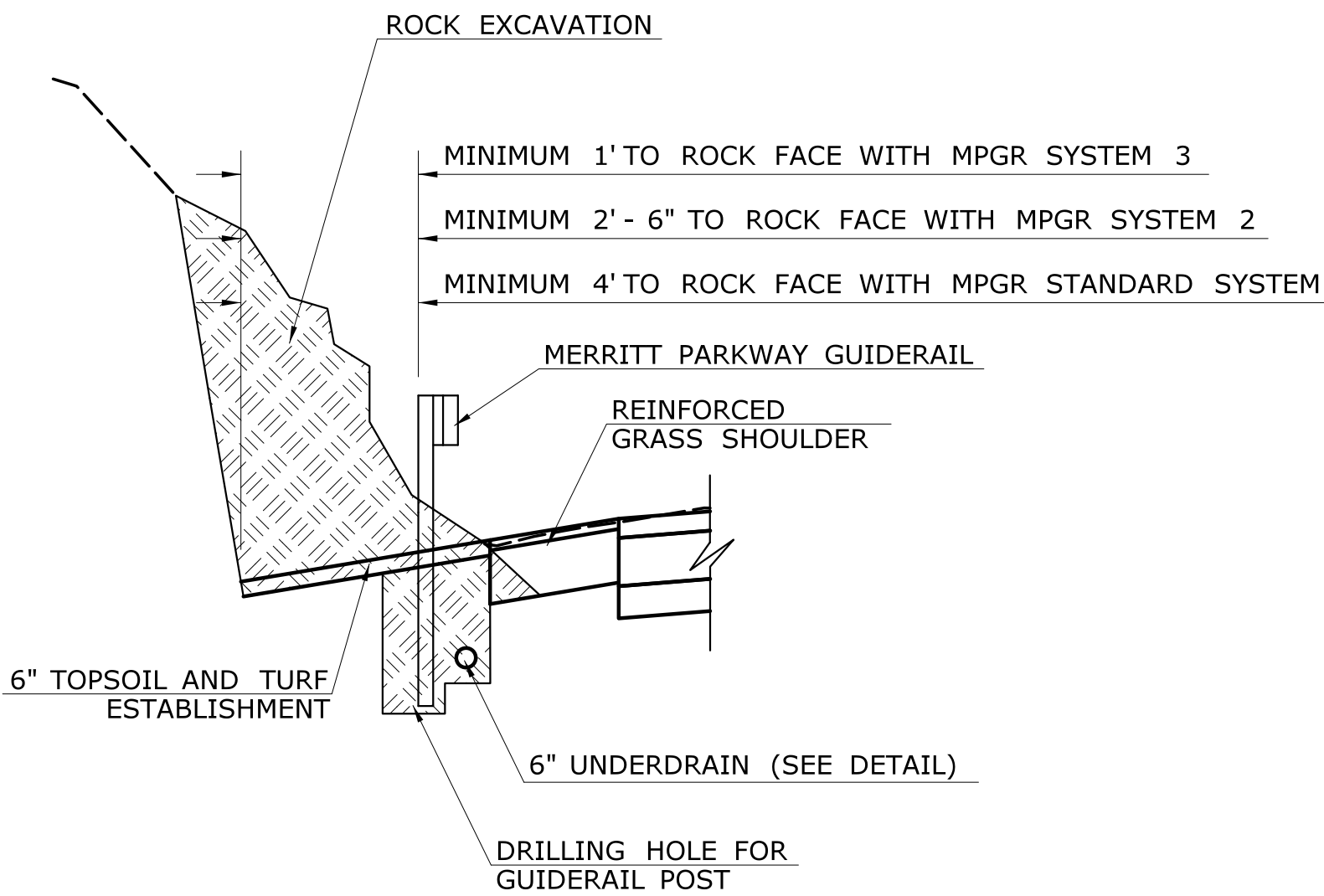
01.03.12



**REINFORCED GRASS SHOULDER
(NORMAL SECTION SHOWN)**

NOTES:

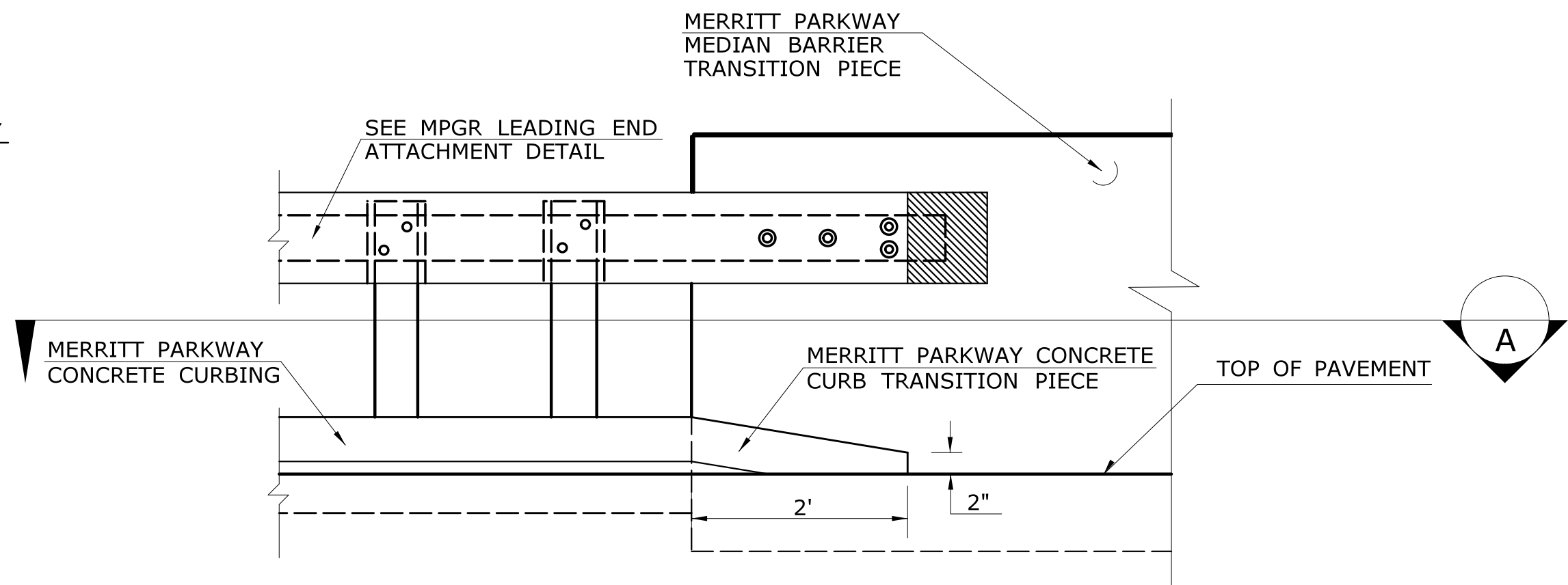
1. THE 2" LAYER OF STRUCTURAL SOIL WITH TURF ESTABLISHMENT SHALL BE INSTALLED IN CONJUNCTION WITH THE 2" FINAL PAVEMENT OVERLAY OPERATIONS.



**ROCK REMOVAL AND
GUIDERAIL INSTALLATION**

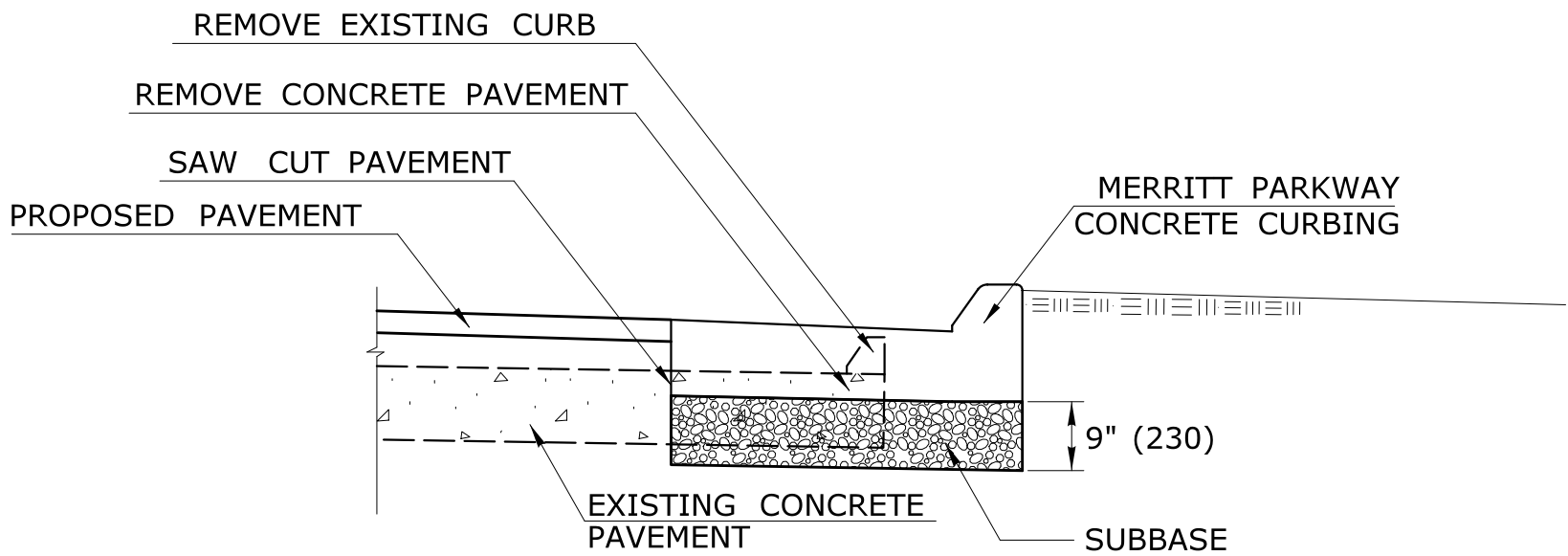
NOTES:

1. PRESPLITTING OF ROCK AND OR THE USE OF EXPLOSIVES SHALL ONLY BE PERMITTED WHERE MORE THAN 4 FEET OF ROCK IS TO REMOVED AS MEASURED FROM THE FACE
2. THE USE OF EXPLOSIVES MAY NOT BE FEASIBLE IN ALL OR ANY AREAS WITHIN THE PROJECT LIMITS. THEREFORE, MECHANICAL REMOVAL OF ROCK FOR THE ITEM "ROCK EXCAVATION" MAY BE REQUIRED.
3. IF EXPLOSIVES ARE USED FOR ROCK EXCAVATION, ALL DRILL HOLES SHALL BE ERADICATED AS PER SPECIFICATIONS.
4. THE QUANTITY OF ROCK EXCAVATION SHALL BE MEASURED FROM THE CROSS SECTIONS PROVIDED, UNLESS OTHERWISE SPECIFICALLY APPROVED PRIOR TO THE OPERATION.
5. UNDERDRAINS SHALL BE INSTALLED AT ROCK CUTS AND GRADED TO DRAIN TO AN ADJACENT DRAINAGE STRUCTURE AS DIRECTED BY THE ENGINEER.

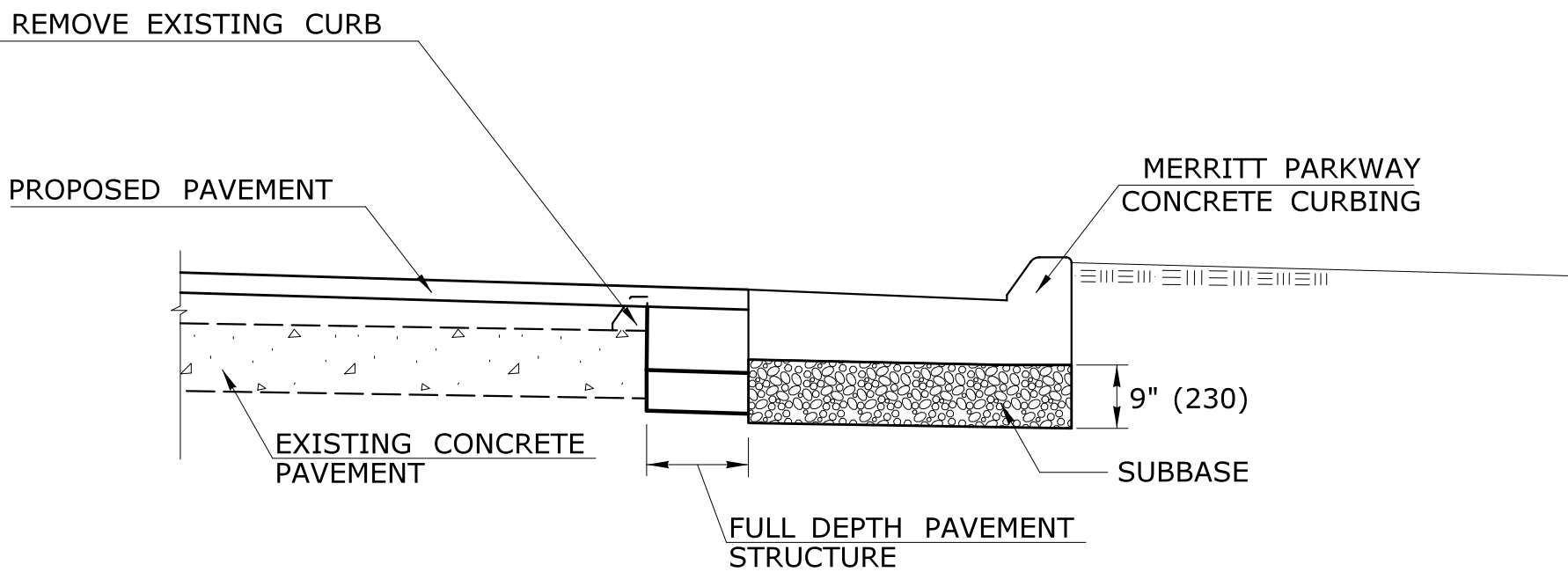


ELEVATION

**MERRITT PARKWAY CONCRETE CURB AT
MERRITT PARKWAY MEDIAN BARRIER LEADING END**



CASE 1

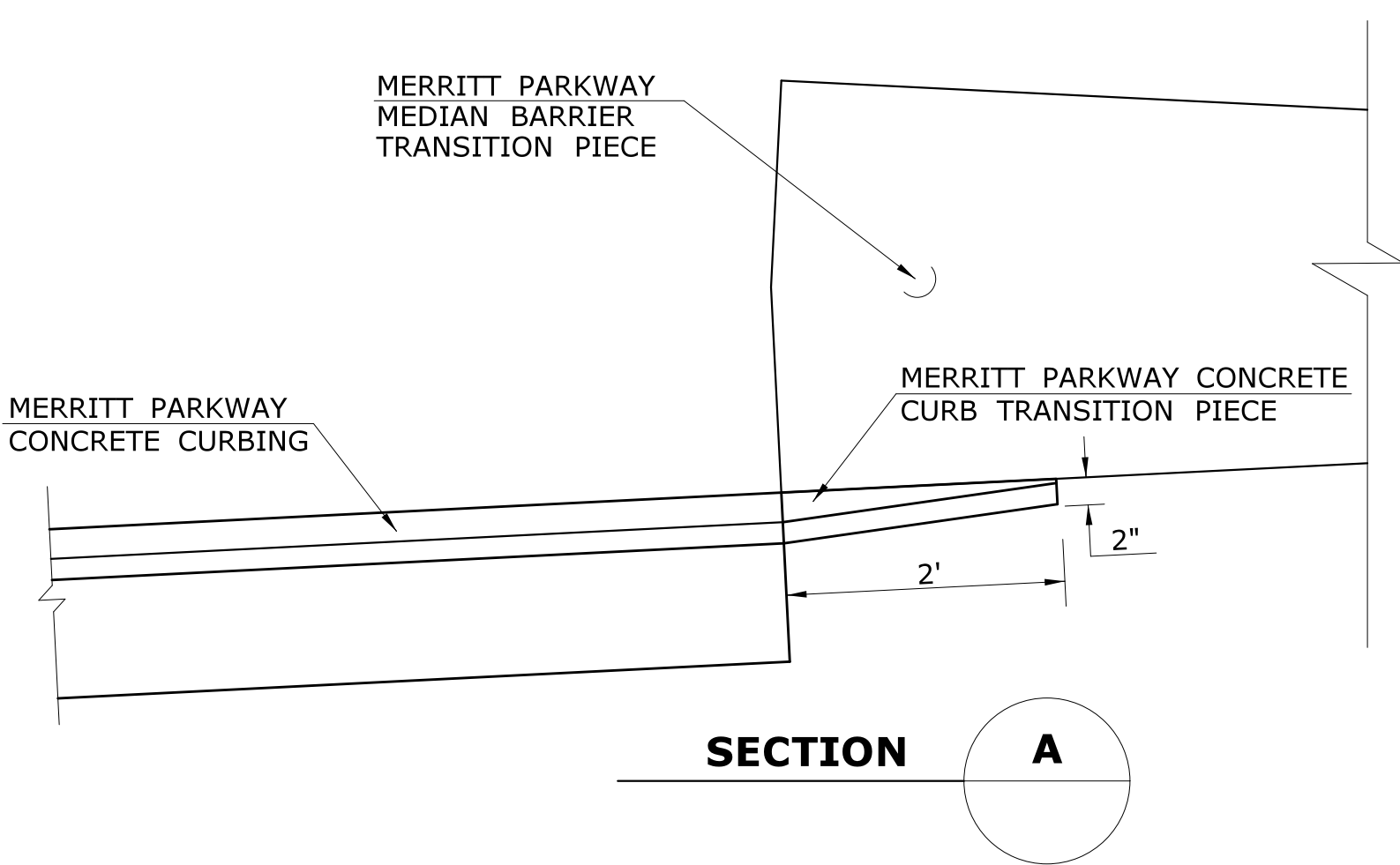


CASE 2

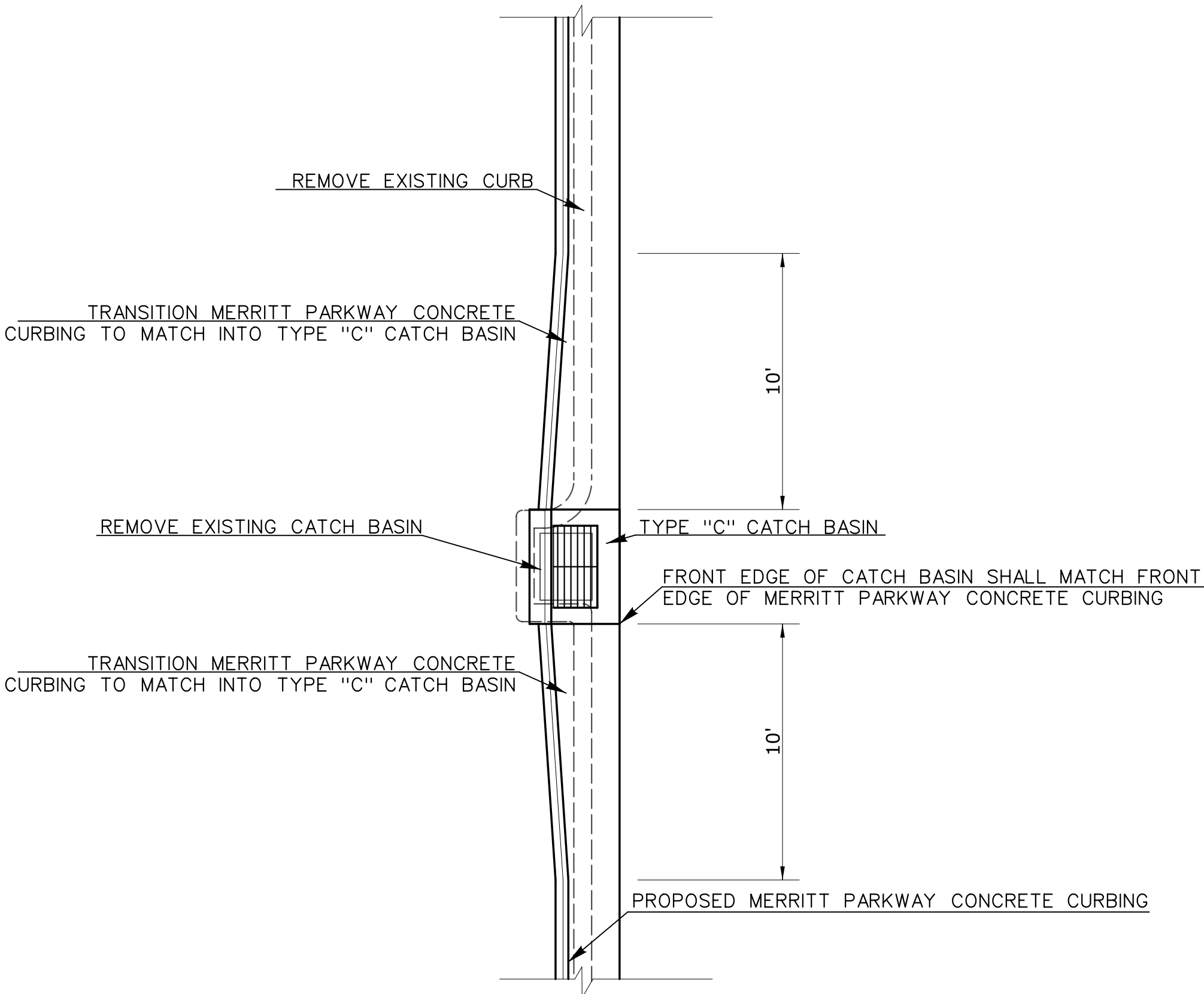
**TYPICAL CONDITIONS FOR PLACEMENT
OF MERRITT PARKWAY CONCRETE CURBING**

NOTES:

1. MERRITT PARKWAY CONCRETE CURBING SHALL BE SLIP FORMED
2. SUBBASE SHALL BE INCLUDED IN THE COST OF "MERRITT PARKWAY CONCRETE CURBING"
3. SAW CUTTING AND REMOVAL OF PAVEMENT (BITUMINOUS AND CONCRETE) SHALL BE INCLUDED IN THE COST OF "MERRITT PARKWAY CONCRETE CURBING"
4. FORMATION OF SUBGRADE AND THE REMOVAL AND EXCAVATION OF ALL MATERIAL NECESSARY TO INSTALL MERRITT PARKWAY CONCRETE CURB AND ASSOCIATED SUBBASE SHALL BE INCLUDED IN THE COST FOR "MERRITT PARKWAY CONCRETE CURBING"
5. INTALLATION OF THE MERRITT PARKWAY CONCRETE CURB TRANSITION PIECE SHALL BE INCLUDED IN THE COST OF OF "MERRITT PARKWAY CONCRETE CURBING"





SECTION A

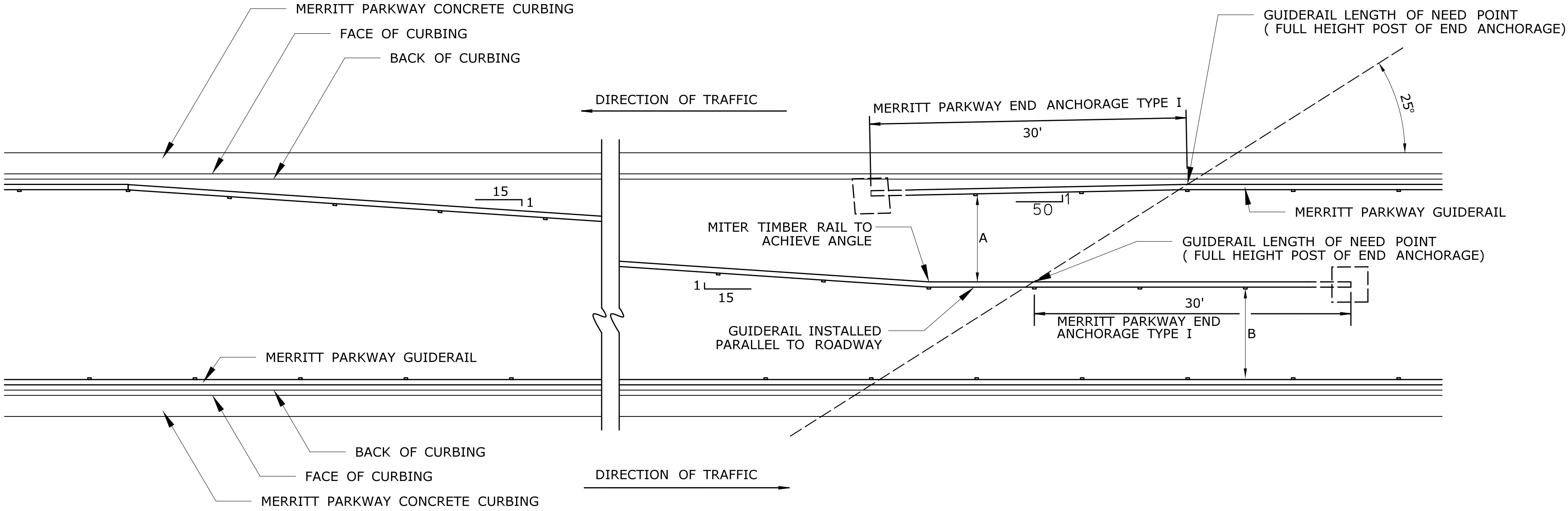


**MERRITT PARKWAY CONCRETE CURBING
AT TYPE "C" CATCH BASIN**

NOTES:

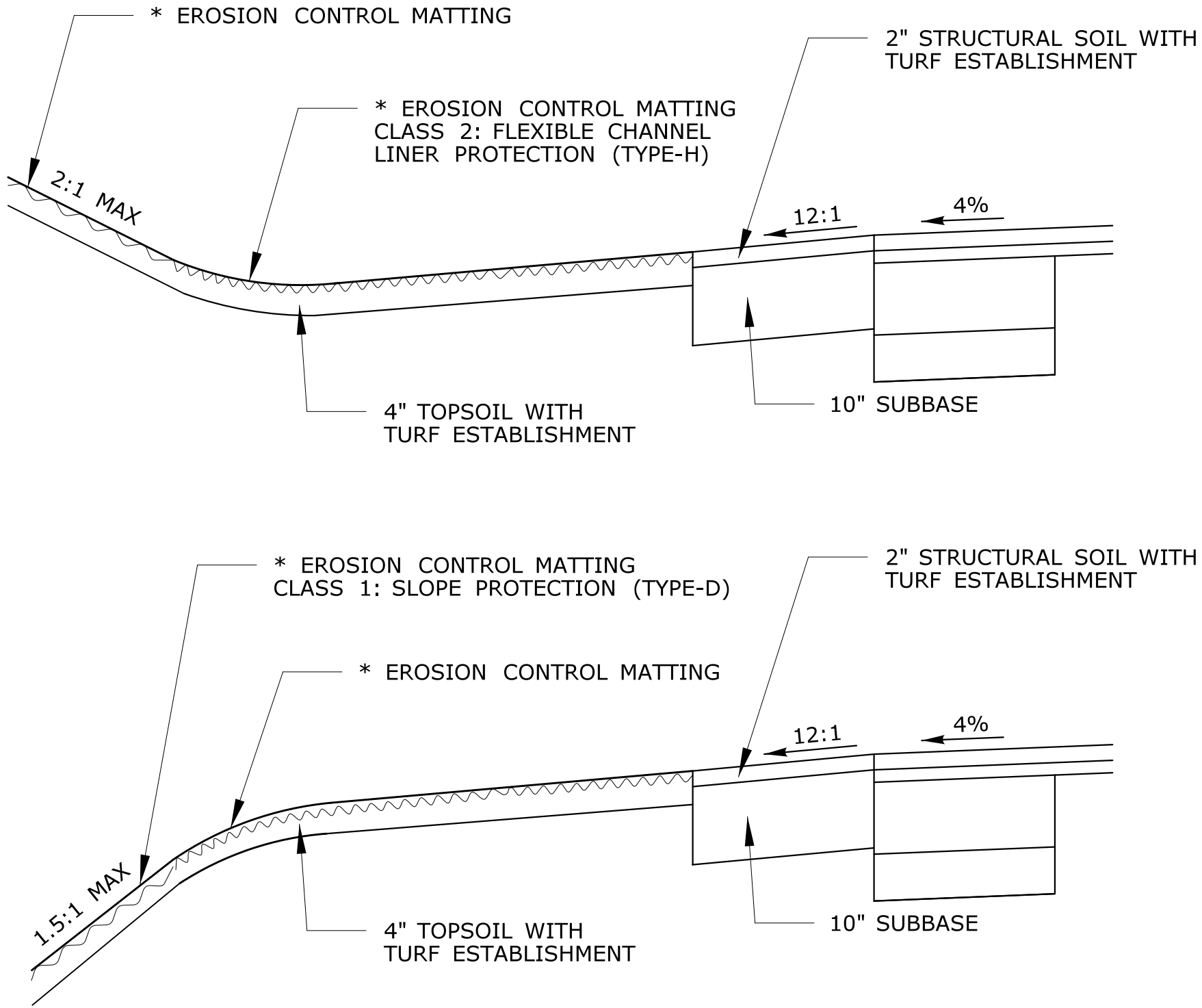
1. THE INTERFACE OF CURBING AND CATCHBASINS SHALL BE FREE OF SNAG POINTS

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				CHECKED BY: -							APPROVED BY: <i>William B. Entell</i>	DRAWING TITLE: MERRITT PARKWAY GUIDERAIL	DRAWING NO. GDS-06
				SCALE AS NOTED									
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 7/18/2016		Filename: ...\\158-211_Guide_06_MPCC_GR-In-rock.dgn							



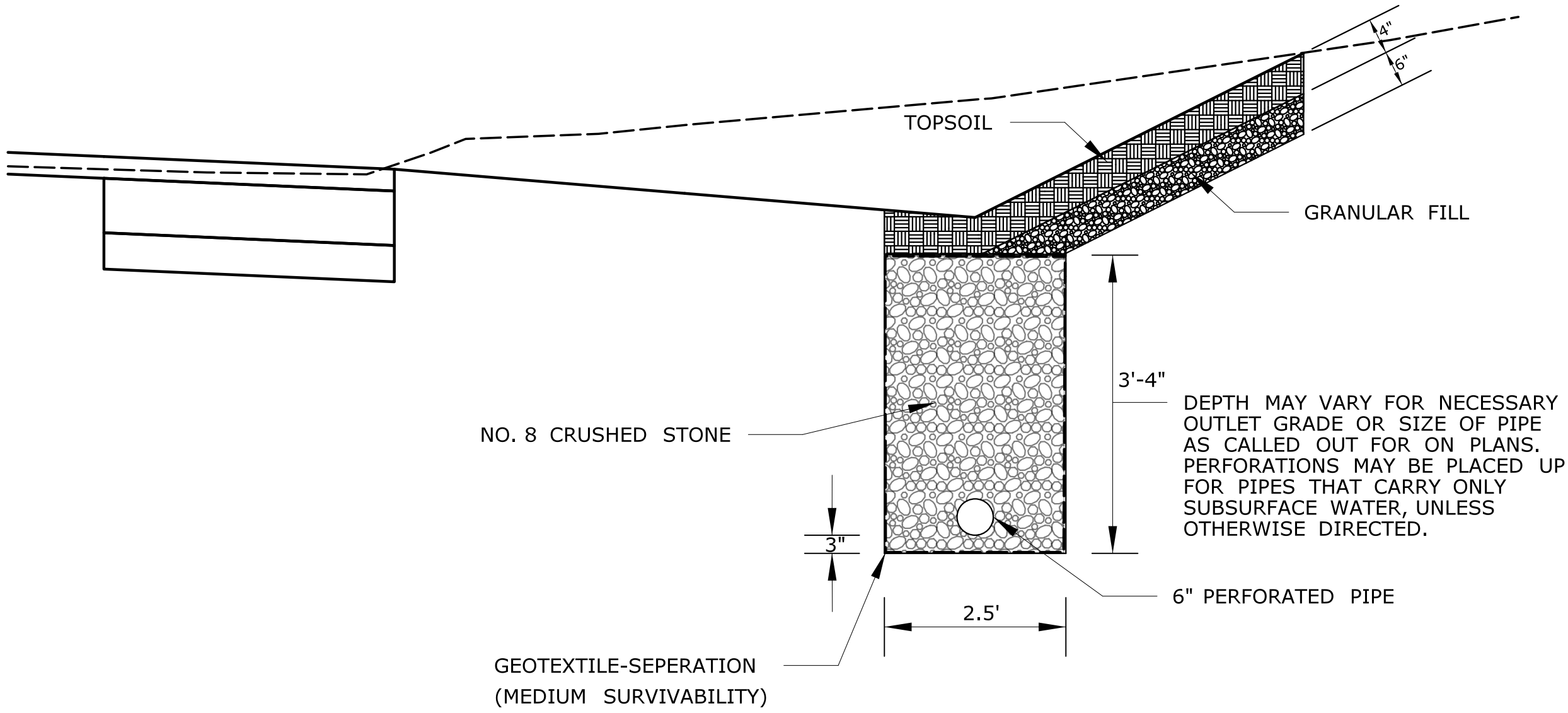
- NOTES:
- 1. DIMENSION "A" SHALL BE EQUAL TO DIMENSION "B" TO PROVIDE THE MAXIMUM WIDTH FOR MAINTENANCE ACCESS TO THE MEDIAN.
 - 2. THE MINIMUM ANGLE MEASURED PARALLEL TO THE ROADWAY TO THE LINE CONNECTING THE FULL HEIGHT POSTS OF THE END ANCHORS SHALL BE 25°, THE PURPOSE OF THIS CONFIGURATION IS TO SHIELD THE END ANCHORAGE LOCATED TOWARD THE MIDDLE OF THE MEDIAN FROM ERRANT VEHICLES.

MERRITT PARKWAY GUIDERAIL MEDIAN OPENING LAYOUT

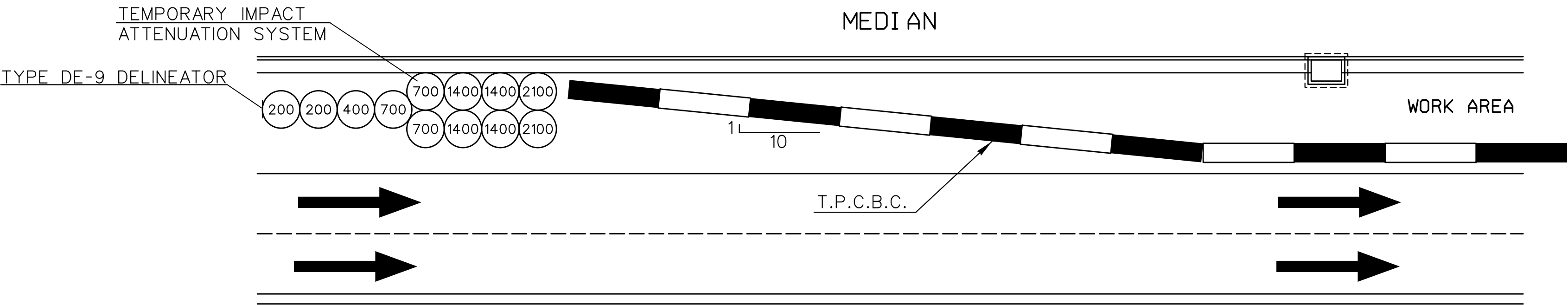


- NOTES:
- 1. CUT SLOPES SHALL NOT EXCEED 2:1
 - 2. FILL SLOPES SHALL NOT EXCEED 1.5:1
EROSION CONTROL MATTING ONLY ALLOWED ON FILL SLOPES GREATER THAN 2:1
 - 3. TREATMENT OF SLOPE SURFACE: USE STANDARD TURF ESTABLISHMENT FOR SLOPES 2:1 OR FLATTER.
- * TO BE PAID FOR AS "EROSION CONTROL MATTING"
TO BE INSTALLED AS DIRECTED BY THE ENGINEER



EROSION CONTROL MATTING

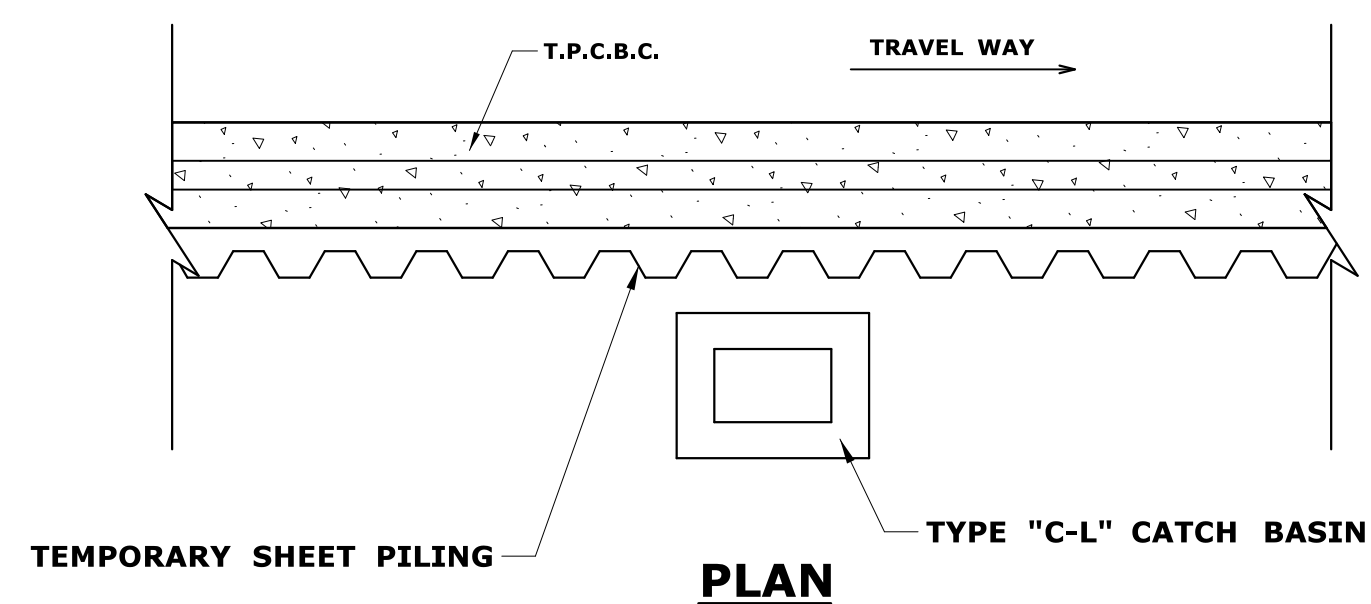
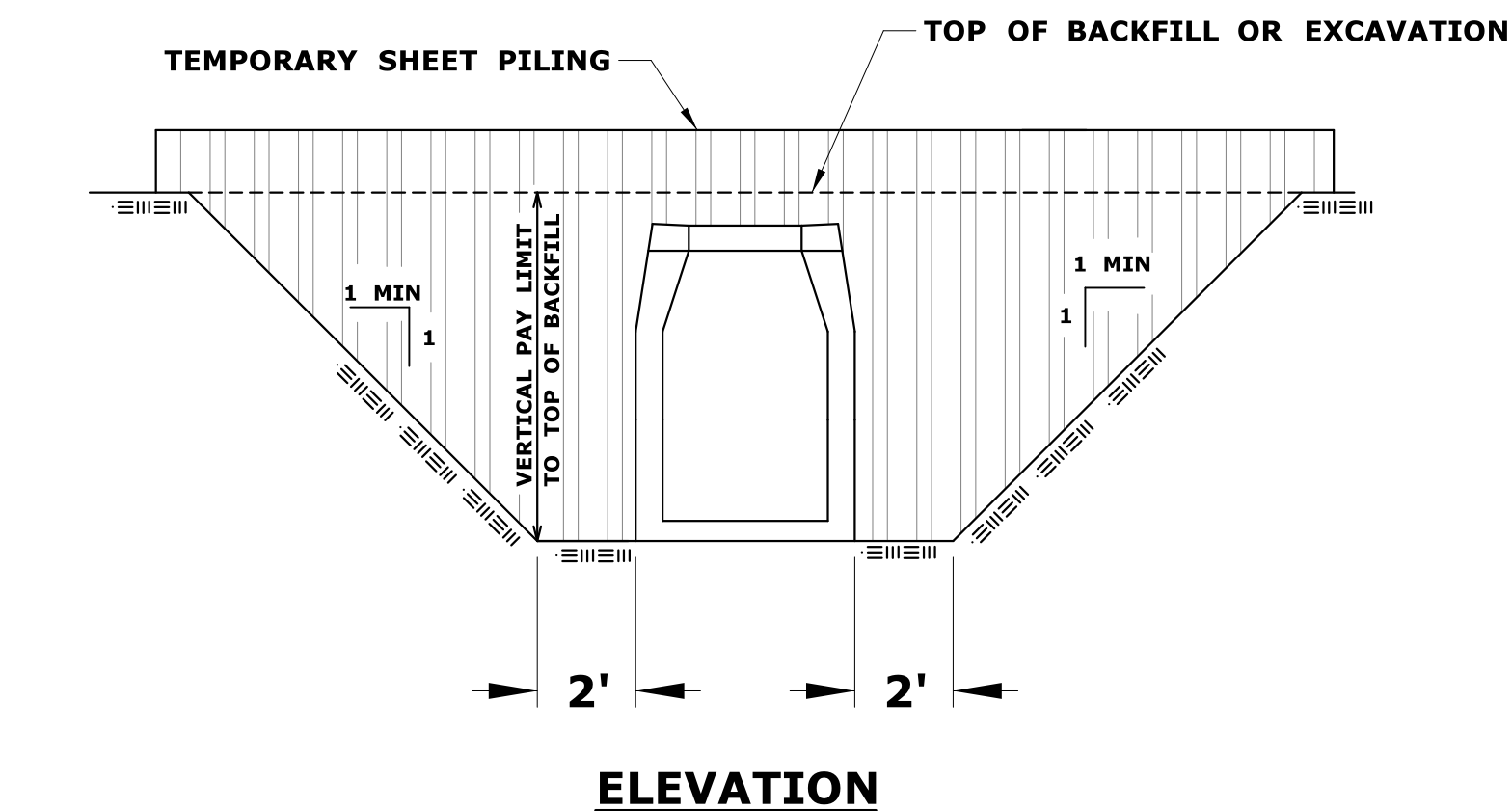
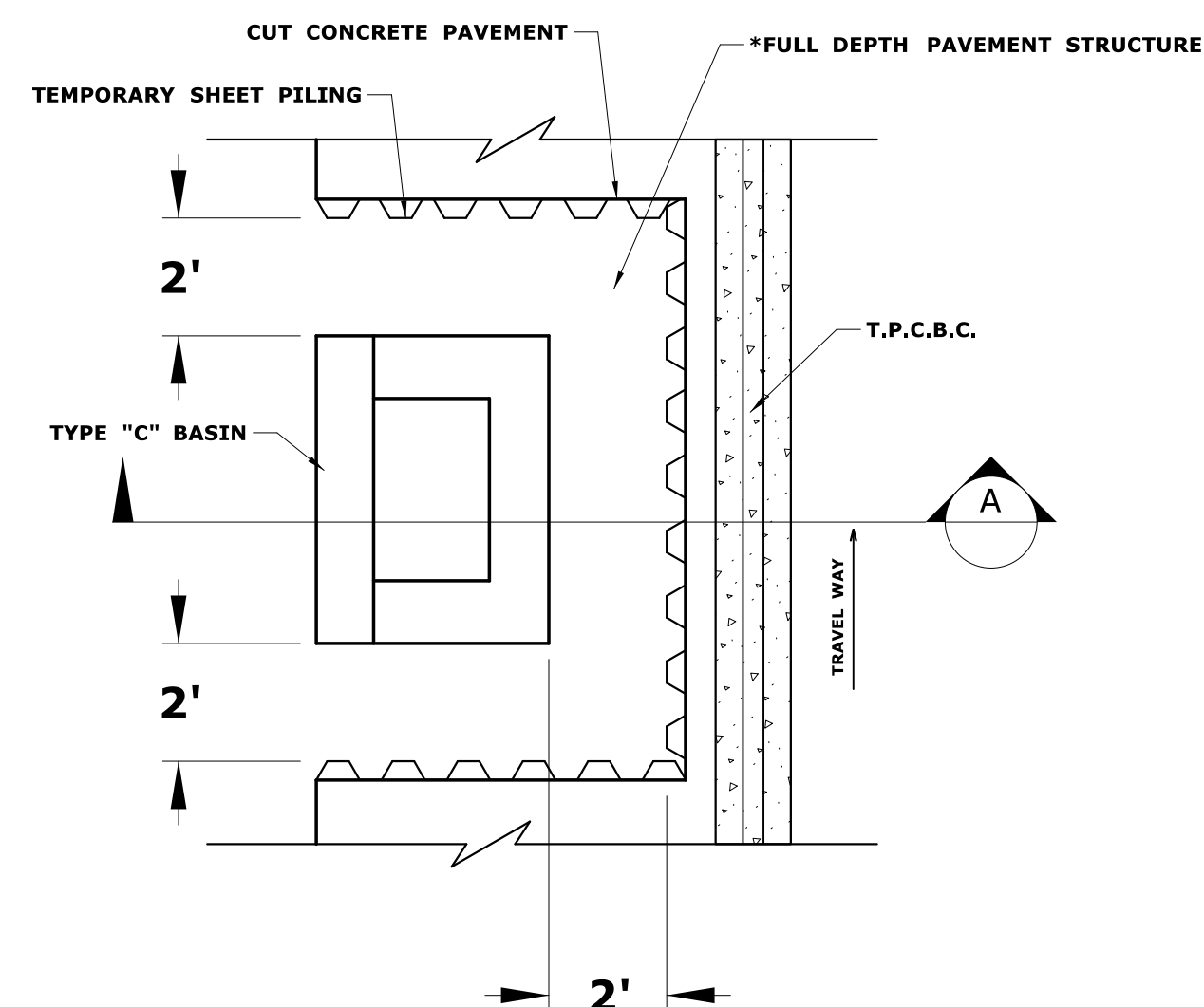
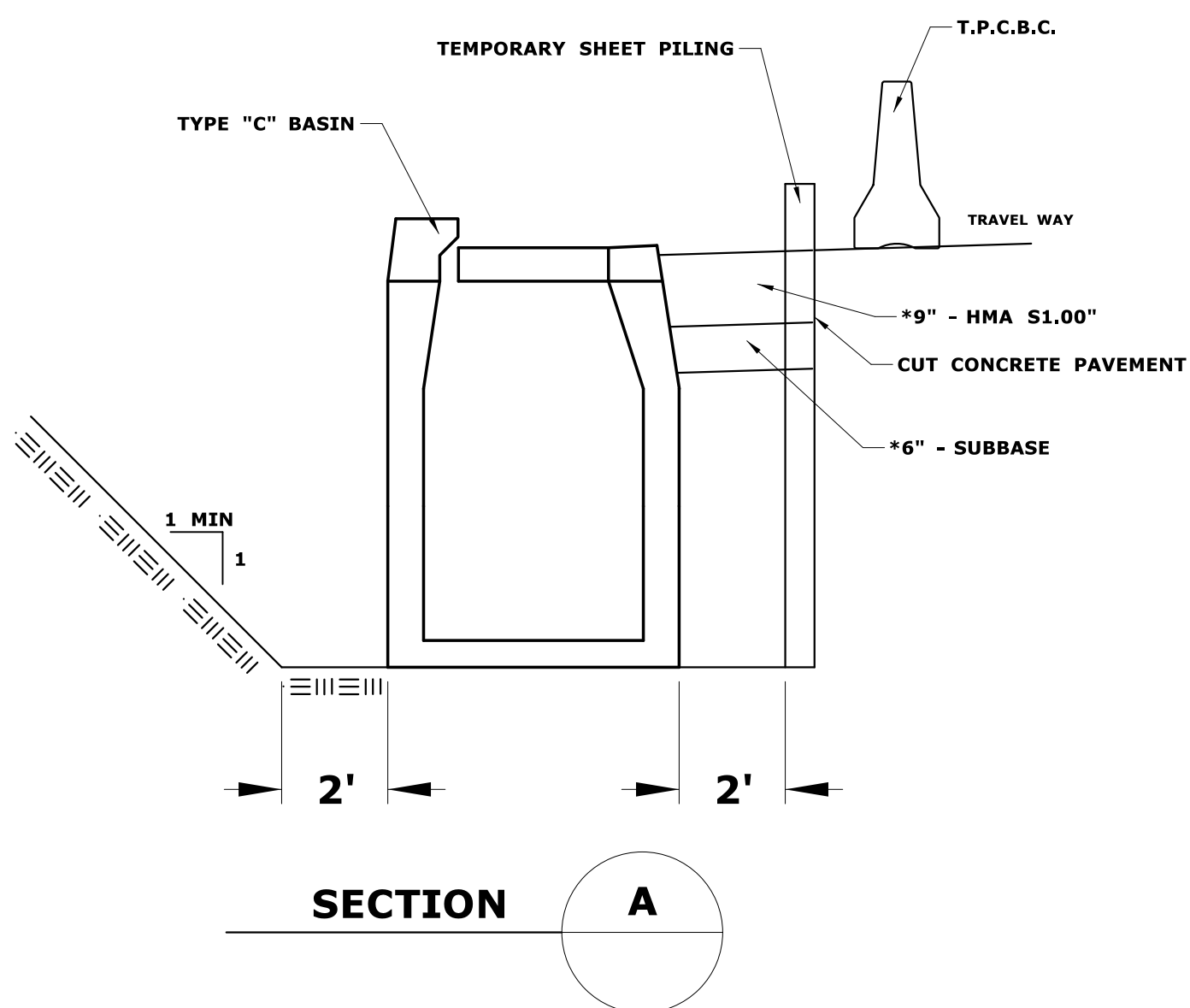


6" UNDERDRAIN TYPICAL SECTION



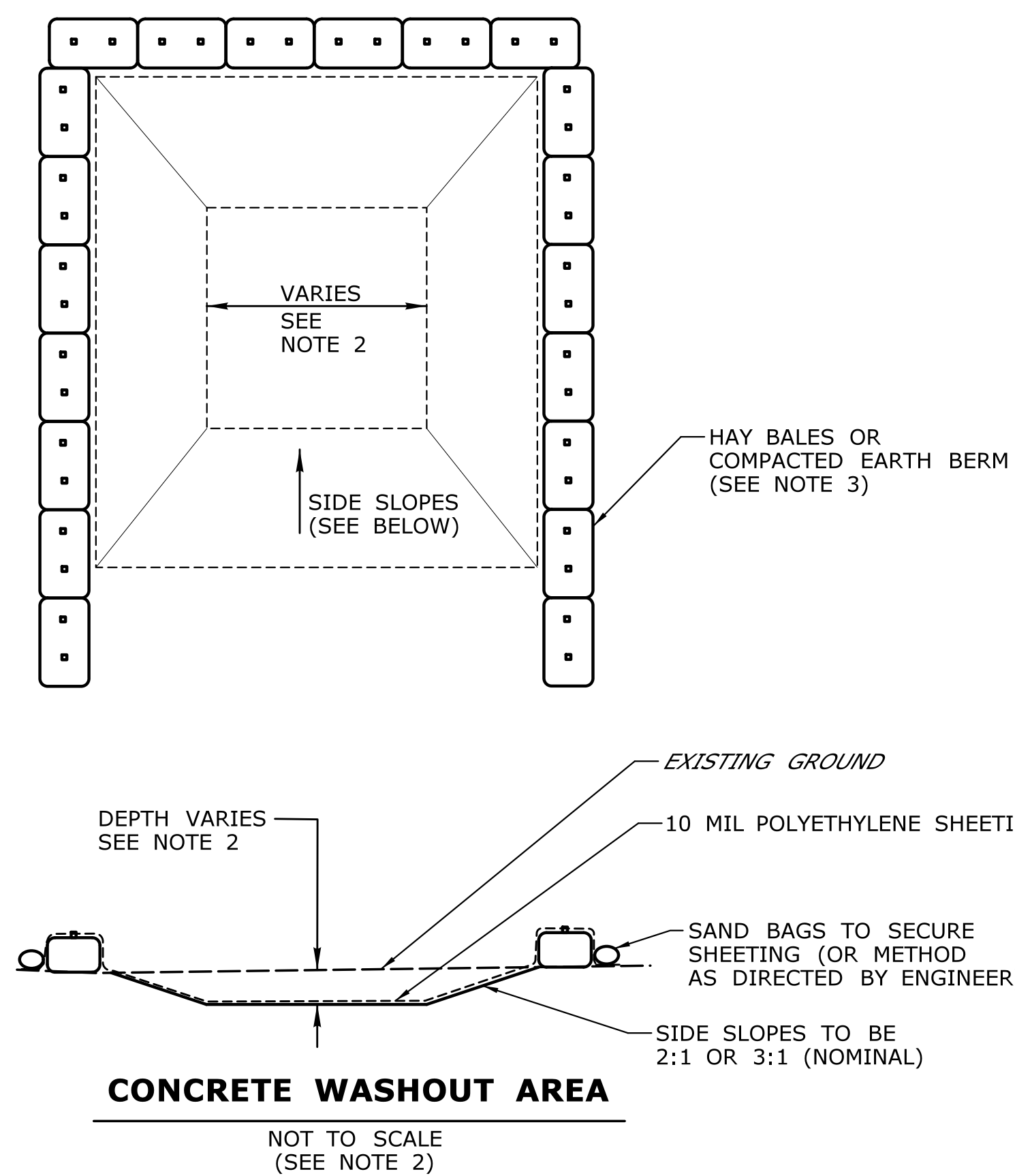
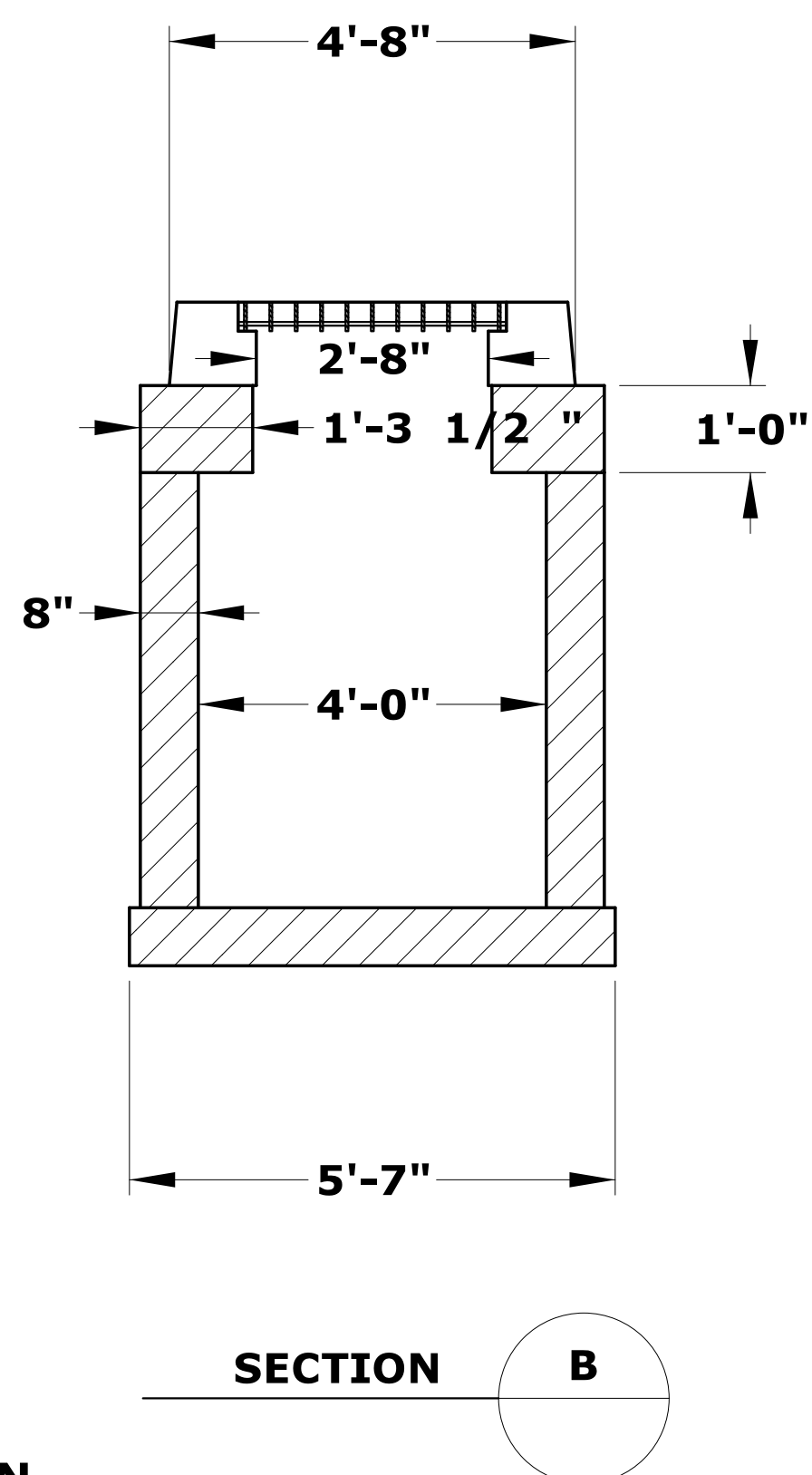
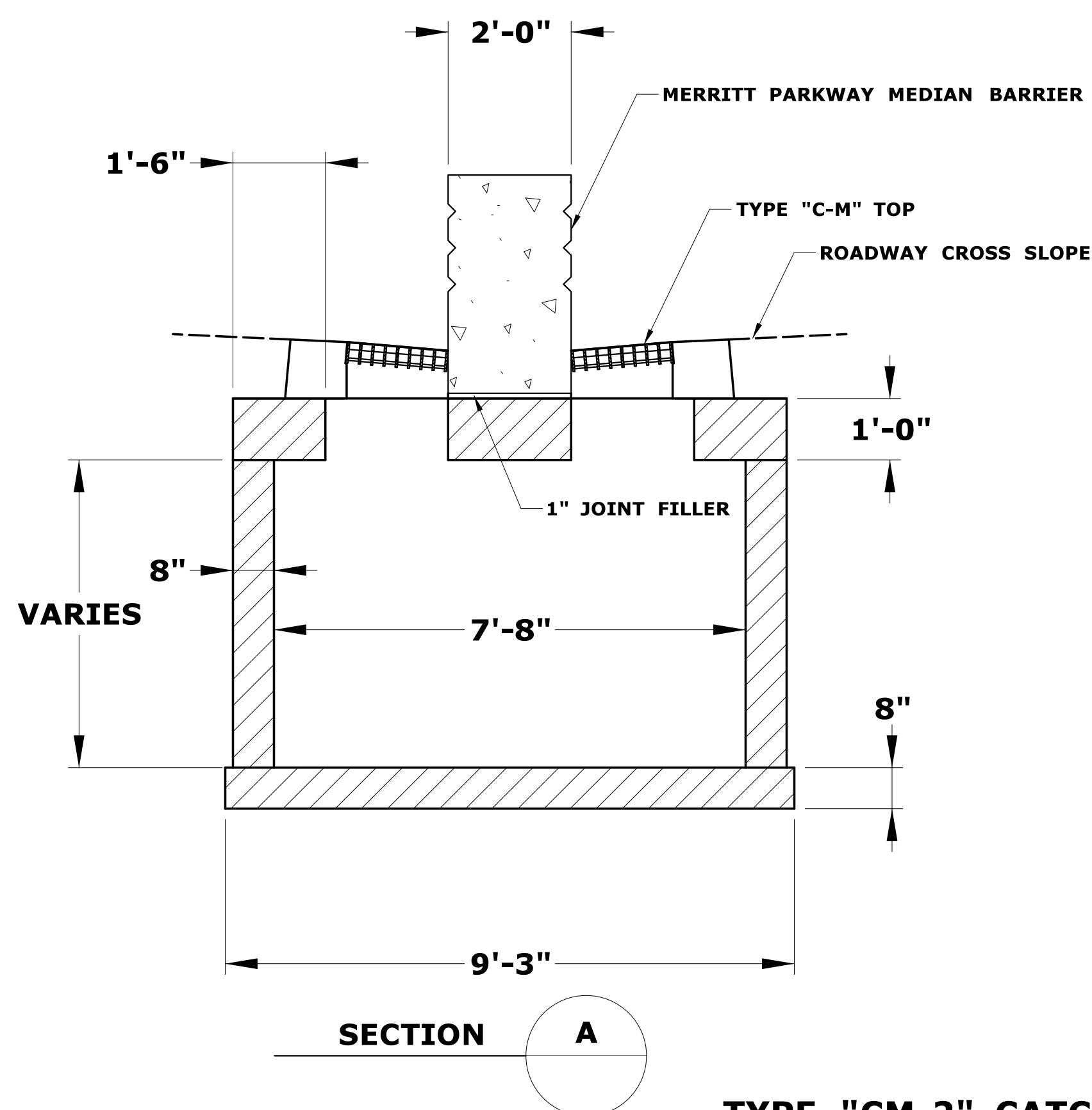
LEADING END OF TEMPORARY BARRIER TREATMENT
(NUMBERS SHOWN IN CIRCLES INDICATE MASS OF SAND IN POUNDS IN EACH BARREL)

				DESIGNER/DRAFTER: -		 STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	SIGNATURE/ BLOCK: OFFICE OF ENGINEERING APPROVED BY: 	PROJECT TITLE: ROUTE 15 SAFETY IMPROVEMENTS, RESURFACING, ENHANCEMENTS, AND BRIDGE IMPROVEMENTS	TOWN: WESTPORT FAIRFIELD	PROJECT NO. 158-211					
				CHECKED BY: -											
				SCALE AS NOTED											
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 7/18/2016		Filename: ...\\158-211_Guide_07_Underdrain_Med_Opening.dgn		DRAWING TITLE: MERRITT PARKWAY GUIDERAIL							
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TEMPORARY SHEET PILING AT MEDIAN CATCH BASIN (TYPE "C" SHOWN)

* SUBSEQUENT TO THE INSTALLATION OF MEDIAN CATCH BASINS FULL DEPTH PAVEMENT STRUCTURE SHALL BE INSTALLED IN THE EXCAVATED AREA ADJACENT TO THE BASIN WITHIN THE ROADWAY FOOTPRINT. THIS WORK SHALL BE COORDINATED WITH THE INSTALLATION OF MERRITT PARKWAY CONCRETE CURBING IF APPLICABLE.



TEMPORARY SHEET PILING AT CATCH BASIN LOCATED IN RIGHT SHOULDER

1. CONCRETE WASHOUT AREA(S) SHALL BE INSTALLED PRIOR TO CONCRETE PLACEMENT ON SITE. THE CONCRETE WASHOUT AREA SHALL BE ENTIRELY SELF-CONTAINED.

2. THE CONTRACTOR SHALL SUBMIT THE DESIGN, LOCATION AND SIZING OF THE CONCRETE WASHOUT AREA(S) WITH THE PROJECT'S EROSION AND SEDIMENTATION CONTROL PLAN AND SHALL BE APPROVED BY THE ENGINEER.

LOCATION: WASHOUT AREA(S) ARE TO BE LOCATED AT LEAST 50 FEET FROM ANY STREAM, WETLAND, STORM DRAINS, OR OTHER SENSITIVE RESOURCE. THE FLOOD CONTINGENCY PLAN MUST ADDRESS THE CONCRETE WASHOUT IF THE WASHOUT IS TO BE LOCATED WITHIN THE FLOODPLAIN.

SIZE: THE WASHOUT MUST HAVE SUFFICIENT VOLUME TO CONTAIN ALL LIQUID AND CONCRETE WASTE GENERATED BY WASHOUT OPERATIONS INCLUDING, BUT NOT LIMITED TO, OPERATIONS ASSOCIATED WITH GROUT AND MORTAR.

3. SURFACE DISCHARGE IS UNACCEPTABLE. THEREFORE, HAY BALES OR OTHER CONTROL MEASURES, AS APPROVED BY THE ENGINEER, SHOULD BE USED AROUND THE PERIMETER OF THE CONCRETE WASHOUT AREA FOR CONTAINMENT.

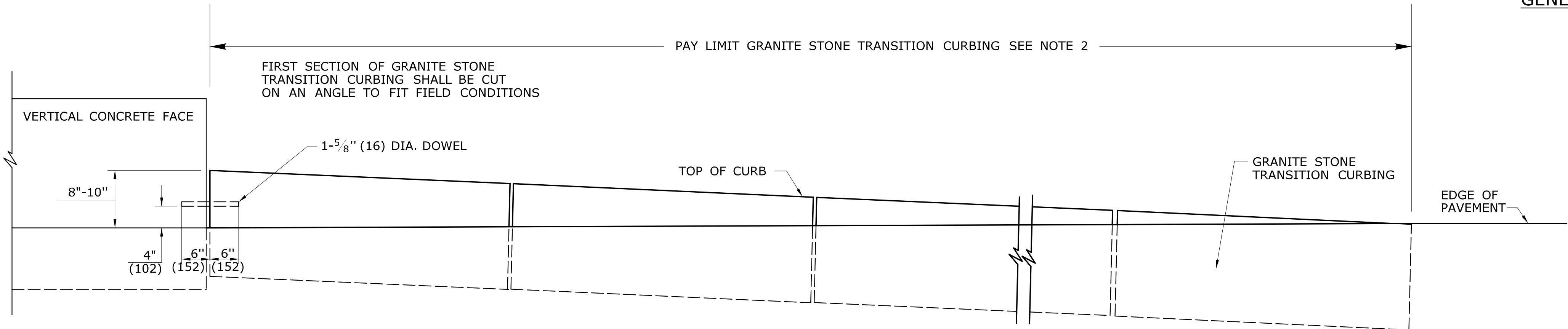
4. SIGNS SHOULD BE PLACED AT THE CONSTRUCTION ENTRANCE, AT THE CONCRETE AREA(S) AND ELSEWHERE AS NECESSARY TO CLEARLY INDICATE THE LOCATION OF THE CONCRETE WASHOUT TO OPERATORS OF CONCRETE TRUCKS AND PUMP RIGS. WASHOUT AREA(S) SHOULD BE FLAGGED WITH SAFETY FENCING OR OTHER APPROVED METHOD.

5. WASHOUT AREA(S) ARE TO BE INSPECTED AT LEAST ONCE A WEEK FOR STRUCTURAL INTEGRITY, ADEQUATE HOLDING CAPACITY AND CHECKED FOR LEAKS, TEARS, OR OVERFLOWS. (AS REQUIRED BY THE CONSTRUCTION SITE ENVIRONMENTAL INSPECTION REPORT) WASHOUT AREA(S) SHOULD BE CHECKED AFTER HEAVY RAINS.

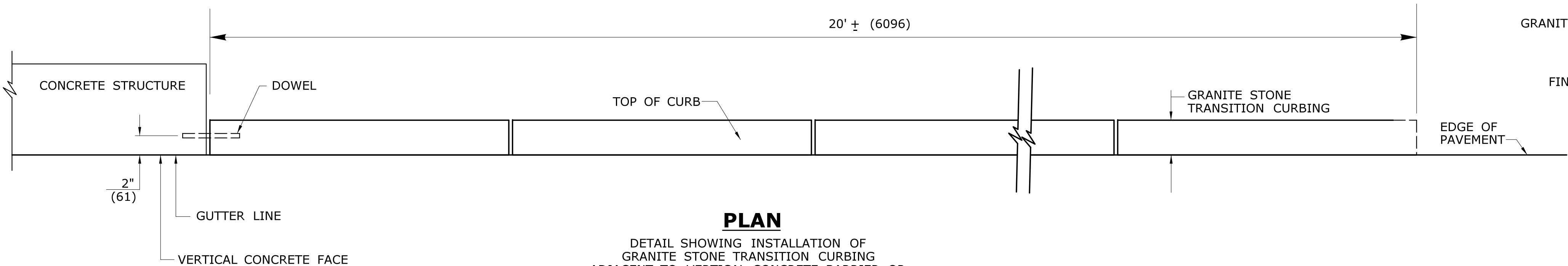
6. HARDENED CONCRETE WASTE SHOULD BE REMOVED AND DISPOSED OF WHEN THE WASTE HAS ACCUMULATED TO HALF OF THE CONCRETE WASHOUT'S HEIGHT. THE WASTE CAN BE STORED AT AN UPLAND LOCATION, AS APPROVED BY THE ENGINEER. ALL CONCRETE WASTE SHALL BE DISPOSED OF IN A MANNER CONSISTENT WITH ALL APPLICABLE LAWS, REGULATIONS, AND GUIDELINES.

7. PAYMENT FOR THIS ITEM IS TO BE INCLUDED UNDER THE GENERAL COST OF THE WORK FOR THE PROJECT, INCLUDING SITE RESTORATION.

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ELEVATION

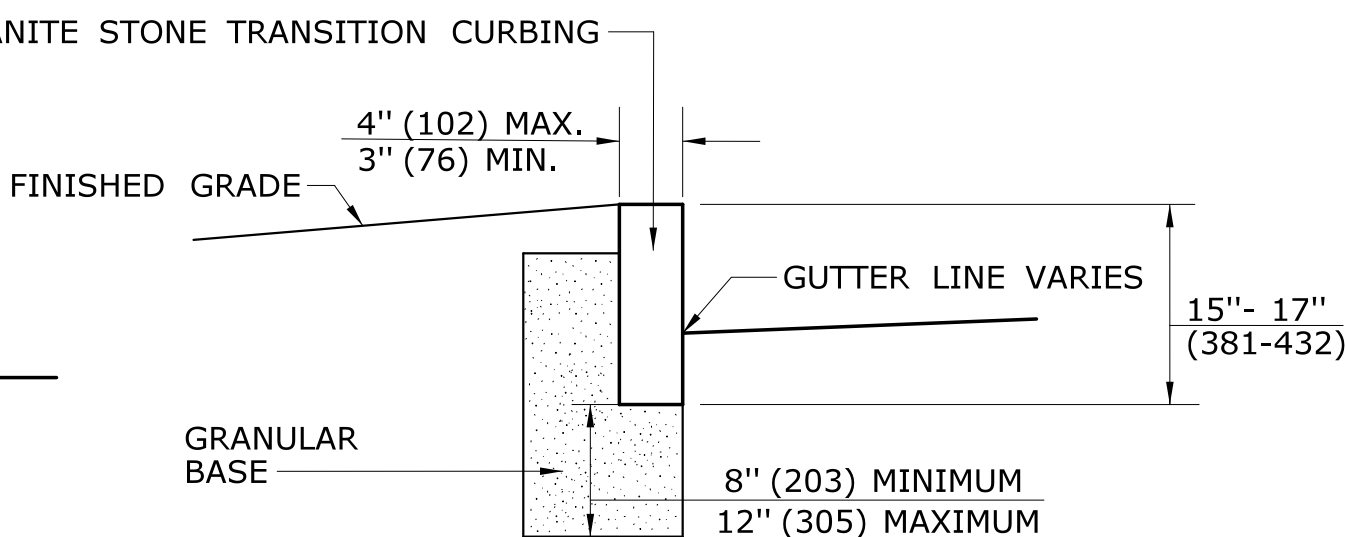


PLAN

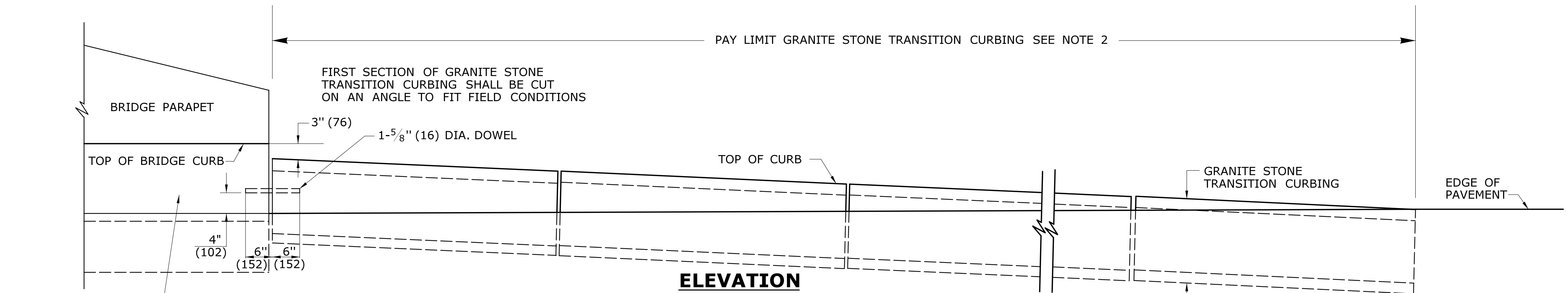
DETAIL SHOWING INSTALLATION OF
GRANITE STONE TRANSITION CURBING
ADJACENT TO VERTICAL CONCRETE BARRIER OR
VERTICAL FACE CONCRETE BRIDGE BUILD-OUT
WITH NO ADJACENT ROADWAY CURBING

GENERAL NOTES:

1. GRANITE STONE TRANSITION CURBING WILL BE INSTALLED AS NOTED ON THE PLANS AND IN ACCORDANCE WITH FORM 816 SECTION 8.13 "STONE CURBING".
2. GRANITE STONE TRANSITION CURBING (INCLUDING DOWEL) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR "GRANITE STONE TRANSITION CURBING".
3. NEW INSTALLATIONS OF THIS CURBING SHALL ONLY BE ALLOWED ON THE MERRITT PARKWAY.
4. GRANITE STONE TRANSITION CURBING SHALL BE INSTALLED TO MATCH THE SLOPE OF SLOPED GRANITE STONE CURBING ON THE BRIDGE. ALL SECTIONS OF THE TRANSITION CURBING SHALL BE 2'-0" (610) LONG.

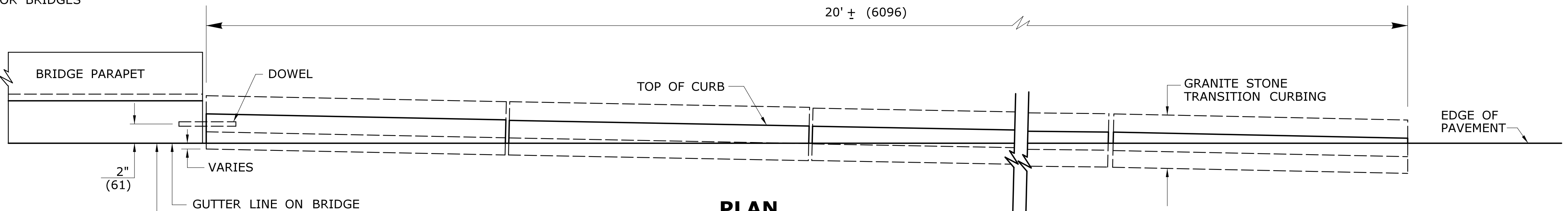


**TYPICAL SECTION SHOWING
INSTALLATION OF
GRANITE STONE TRANSITION CURBING
AT VERTICAL FACE**



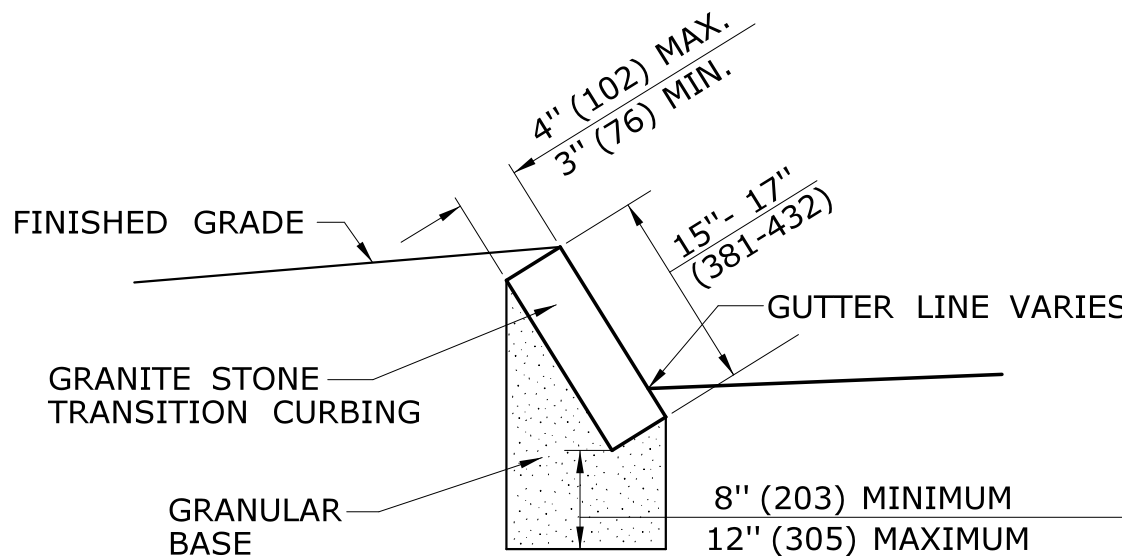
ELEVATION

SLOPED GRANITE STONE
CURBING FOR BRIDGES



PLAN

DETAIL SHOWING INSTALLATION OF
GRANITE STONE TRANSITION CURBING
ADJACENT TO BRIDGE CURBING WITH
NO ADJACENT ROADWAY CURBING



**TYPICAL SECTION SHOWING
INSTALLATION OF
GRANITE STONE TRANSITION CURBING
AT SLOPED GRANITE BRIDGE CURBING**

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

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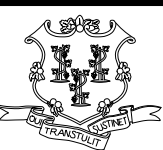
THE INFORMATION, INCLUDING ESTIMATED
QUANTITIES OF WORK, SHOWN ON THESE
SHEETS IS BASED ON LIMITED
INVESTIGATIONS BY THE STATE AND IS
IN NO WAY WARRANTED TO INDICATE
THE CONDITIONS OF ACTUAL QUANTITIES
OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 7/18/2016

DESIGNER/DRAFTER:

CHECKED BY:

SCALE AS NOTED



**STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION**

Filename: ..\158-211_Guide_09_Granite_transition_curb.dgn



SIGNATURE/
BLOCK:

OFFICE OF ENGINEERING

APPROVED BY:

Willie B. Hall

PROJECT TITLE:

**ROUTE 15 SAFETY IMPROVEMENT,
RESURFACING, ENHANCEMENTS,
AND BRIDGE IMPROVEMENTS**

TOWN:

**WESTPORT
FAIRFIELD**

DRAWING TITLE:

**MERRITT PARKWAY
GUIDERAIL**

PROJECT NO.

158-211

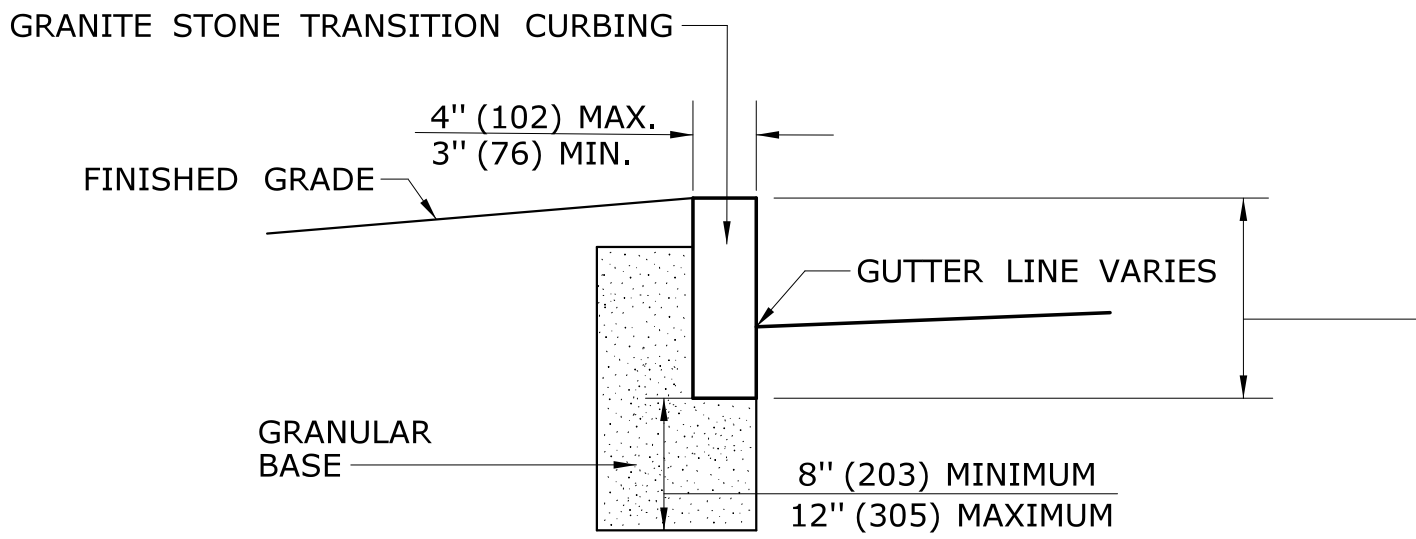
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GDS-09

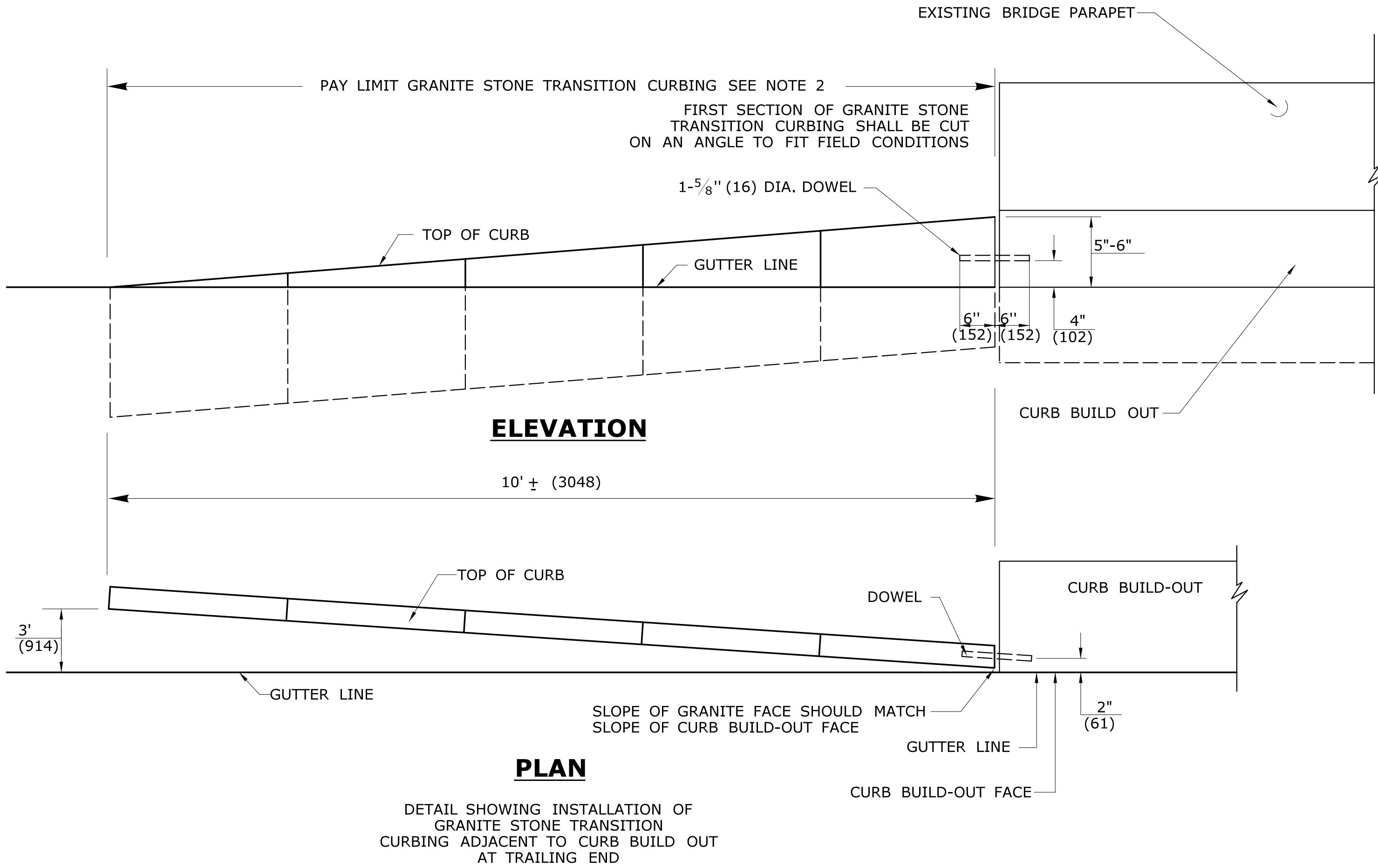
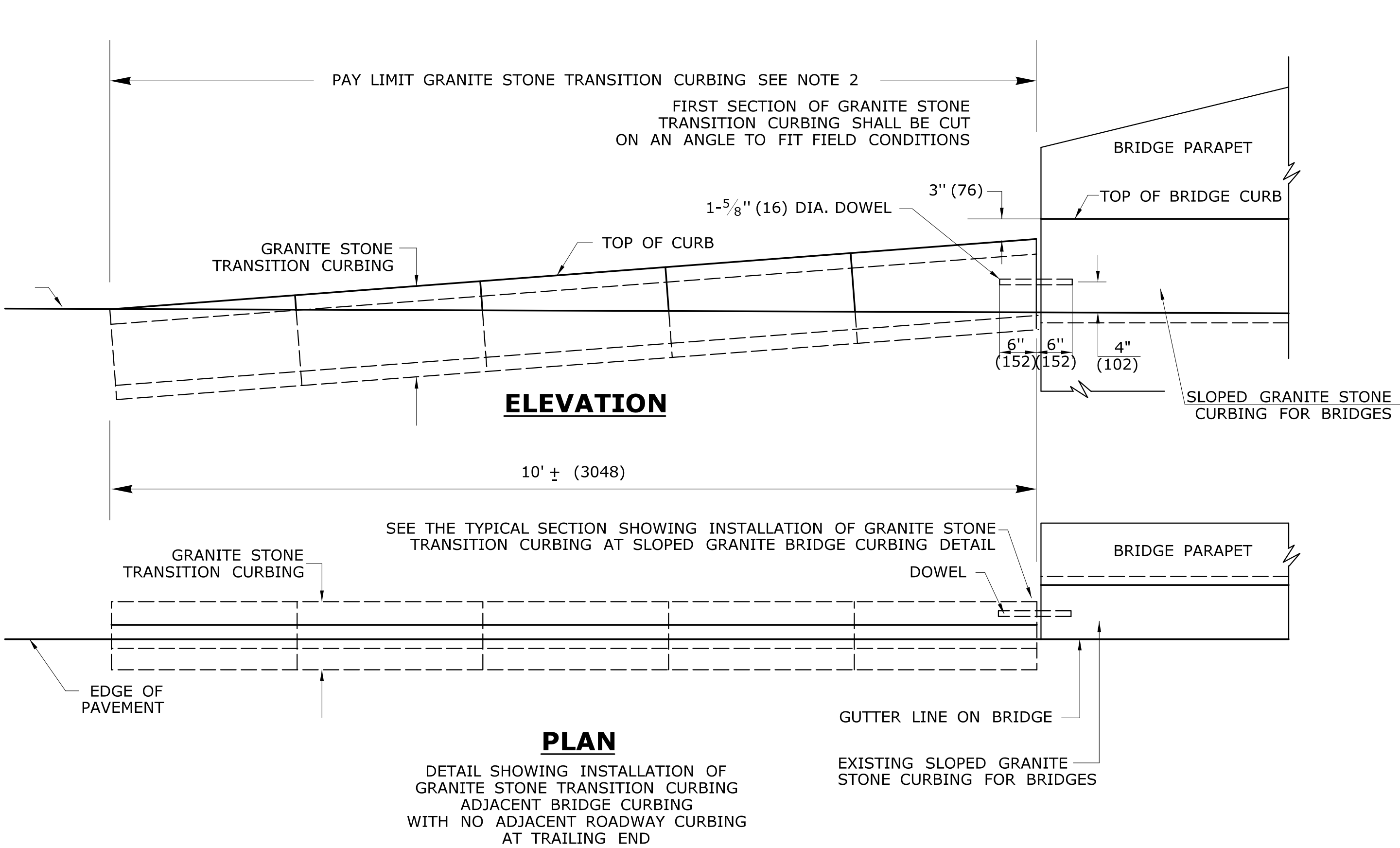
SHEET NO.

01.03.16

- GENERAL NOTES:**
1. GRANITE STONE TRANSITION CURBING WILL BE INSTALLED AS NOTED ON THE PLANS AND IN ACCORDANCE WITH FORM 816 SECTION 8.13 "STONE CURBING".
 2. GRANITE STONE TRANSITION CURBING (INCLUDING DOWEL) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR "GRANITE STONE TRANSITION CURBING".
 3. NEW INSTALLATIONS OF THIS CURBING SHALL ONLY BE ALLOWED ON THE MERRITT PARKWAY.
 4. GRANITE STONE TRANSITION CURBING SHALL BE INSTALLED TO MATCH THE SLOPE OF SLOPED GRANITE STONE CURBING ON THE BRIDGE. ALL SECTIONS OF THE TRANSITION CURBING SHALL BE 2'-0" (610) LONG.

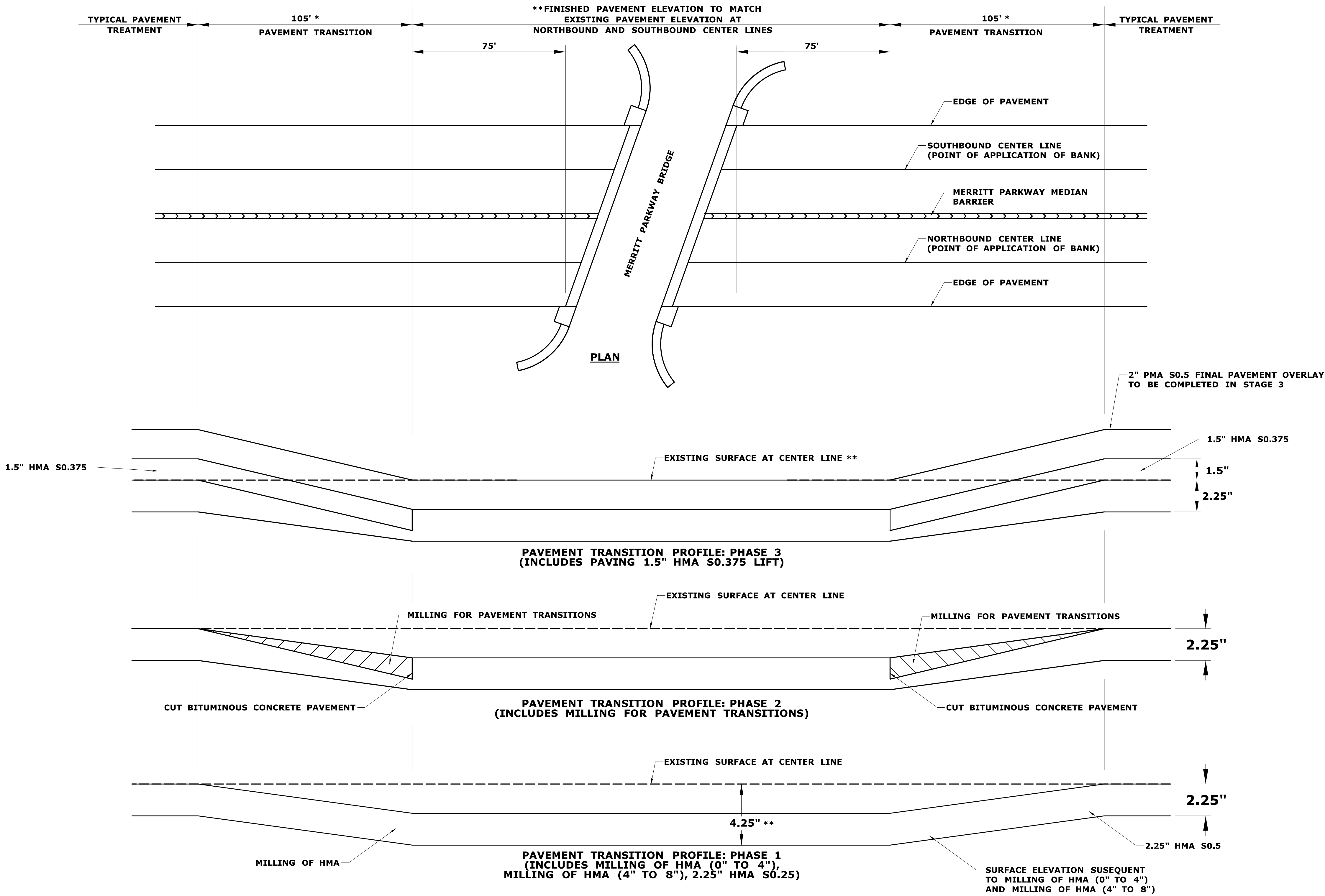


**TYPICAL SECTION SHOWING
INSTALLATION OF
GRANITE STONE TRANSITION CURBING
AT VERTICAL FACE**



				DESIGNER/DRAFTER: CHECKED BY: SCALE AS NOTED		SIGNATURE/ BLOCK: OFFICE OF ENGINEERING APPROVED BY: <i>Willie B. Hall</i>		PROJECT TITLE: ROUTE 15 SAFETY IMPROVEMENTS, RESURFACING, ENHANCEMENTS, AND BRIDGE IMPROVEMENTS		TOWN: WESTPORT FAIRFIELD DRAWING TITLE: MERRITT PARKWAY GUIDERAIL		PROJECT NO. 158-211 DRAWING NO. GDS-10 SHEET NO. 01.03.17	
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 7/18/2016		Filename: ...\\158-211_Guide_10_GraniteTranCurb_2.dgn							



TYPICAL PAVEMENT TRANSITION AT BRIDGE
UNDERPASS AND OVERPASS

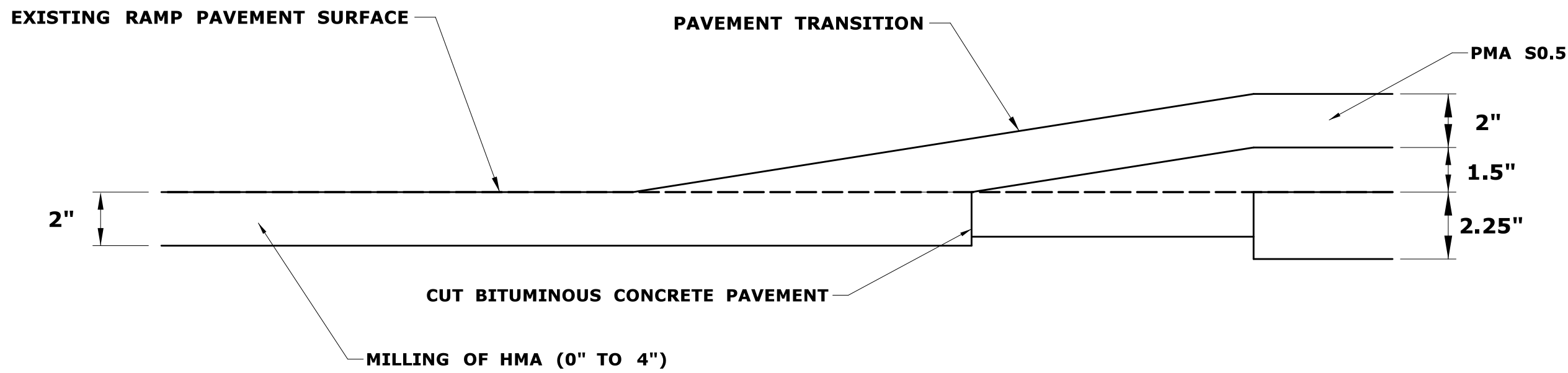


- NOTES:
1. PAVEMENT TRANSITIONS AT BRIDGES ARE REFLECTED IN THE CROSS SECTION SHEETS.
 2. ROADWAY CROSS SLOPE INFORMATION AT BRIDGES IS PROVIDED IN THE TYPICAL SECTIONS, SUPERELEVATION DIAGRAMS AND CROSS SECTION SHEETS.
 3. PAVEMENT TRANSITIONS SHALL BE INCLUDED AT ALL BRIDGES LOCATED WITHIN THE PROJECT LIMITS
 4. AREAS RECEIVING MILLING FOR PAVEMENT TRANSITIONS SHALL BE OVERLAYED WITH 1.5" HMA S0.375 IN THE SAME NIGHT.

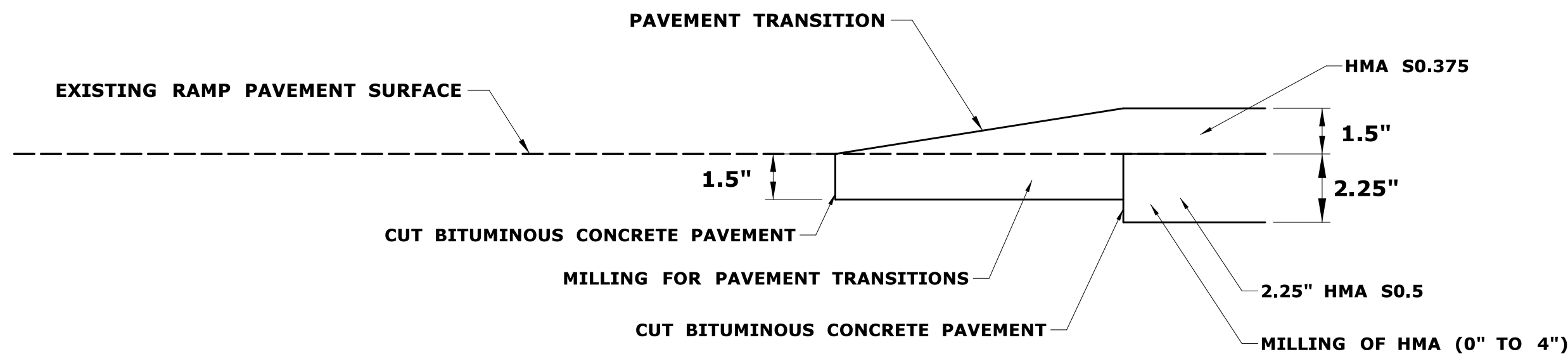
TRANSITION PROFILES

*(WORK TO BE COMPLETED DURING CROSS SLOPE CORRECTION STAGE)
**(PROFILES REFLECT CENTERLINE LOCATION (POINT OF APPLICATION OF GRADE))

				DESIGNER/DRAFTER: - CHECKED BY: - SCALE AS NOTED		 STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION Filename: ...\\158-211_Guide_11_TrackPad_PavTran.dgn	SIGNATURE/ BLOCK: OFFICE OF ENGINEERING APPROVED BY: 	PROJECT TITLE: ROUTE 15 SAFETY IMPROVEMENTS, RESURFACING, ENHANCEMENTS, AND BRIDGE IMPROVEMENTS	TOWN: WESTPORT FAIRFIELD DRAWING TITLE: MERRITT PARKWAY GUIDERAIL	PROJECT NO. 158-211 DRAWING NO. GDS-11 SHEET NO. 01.03.18					
				THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.											
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 7/18/2016											

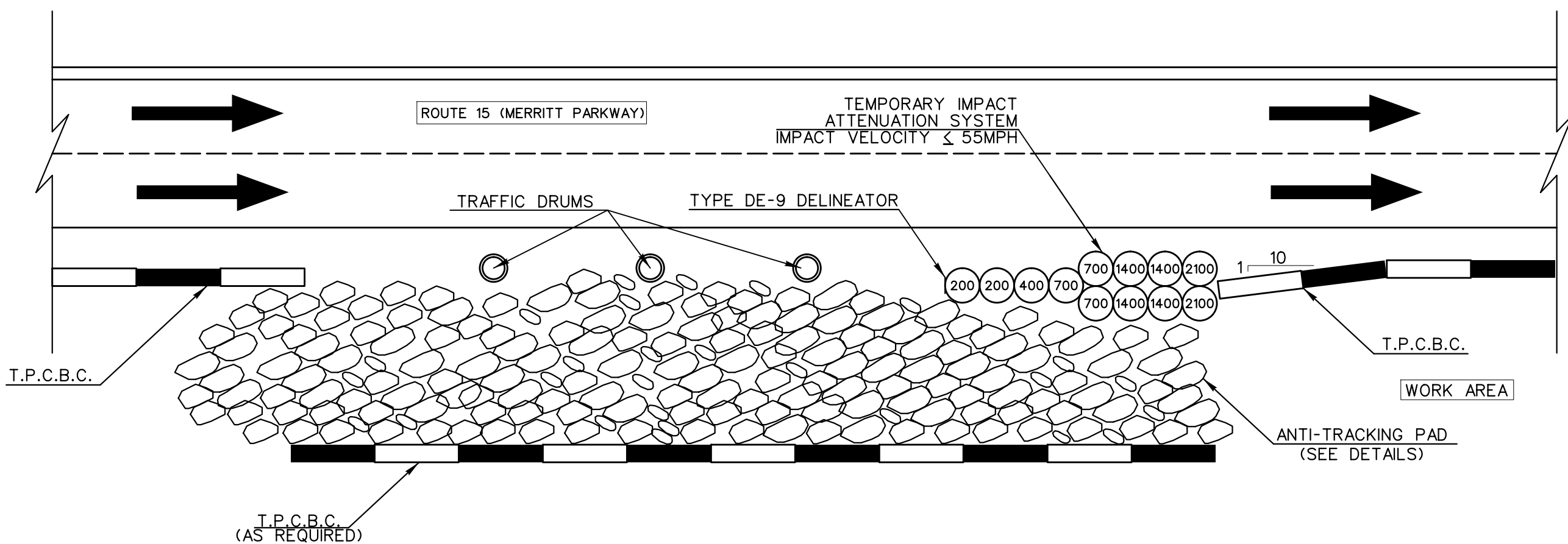


PAVEMENT TRANSITION PROFILE: PHASE 2
(COMPLETED DURING STAGE 3)



PAVEMENT TRANSITION PROFILE: PHASE 1
(COMPLETED DURING THE CROSS SLOPE CORRECTION STAGE)

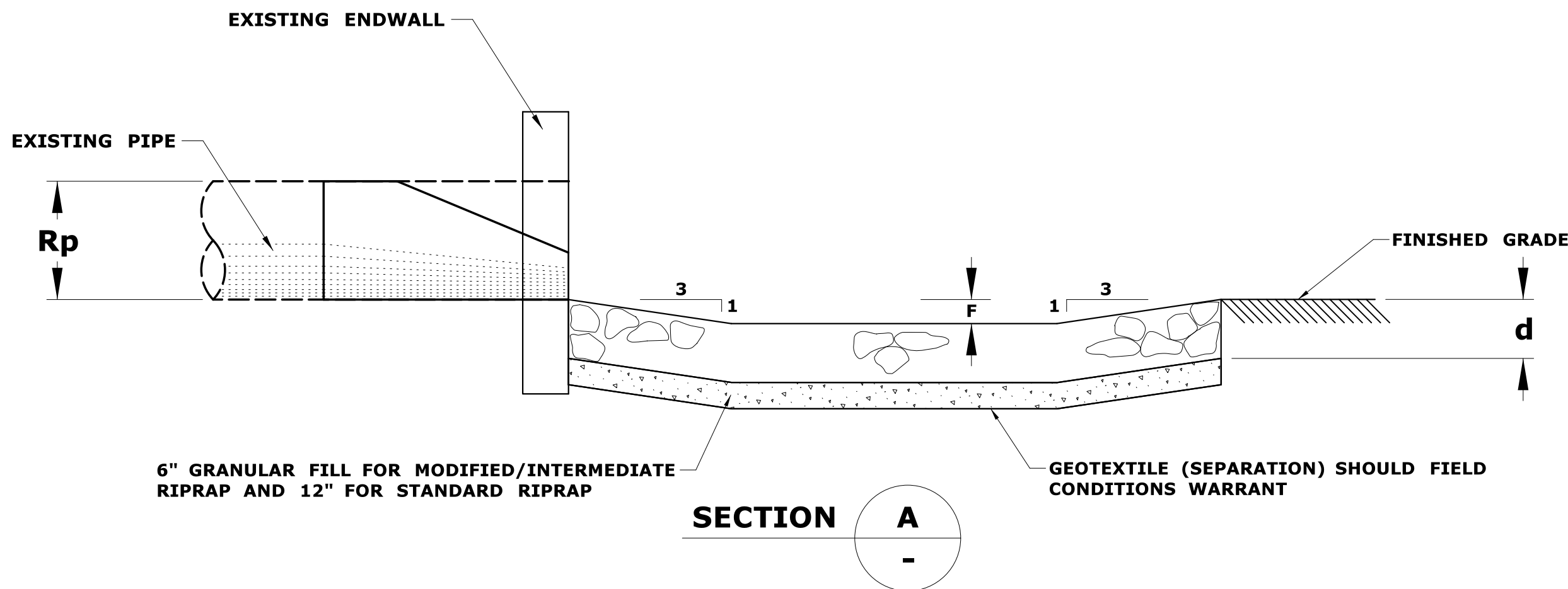
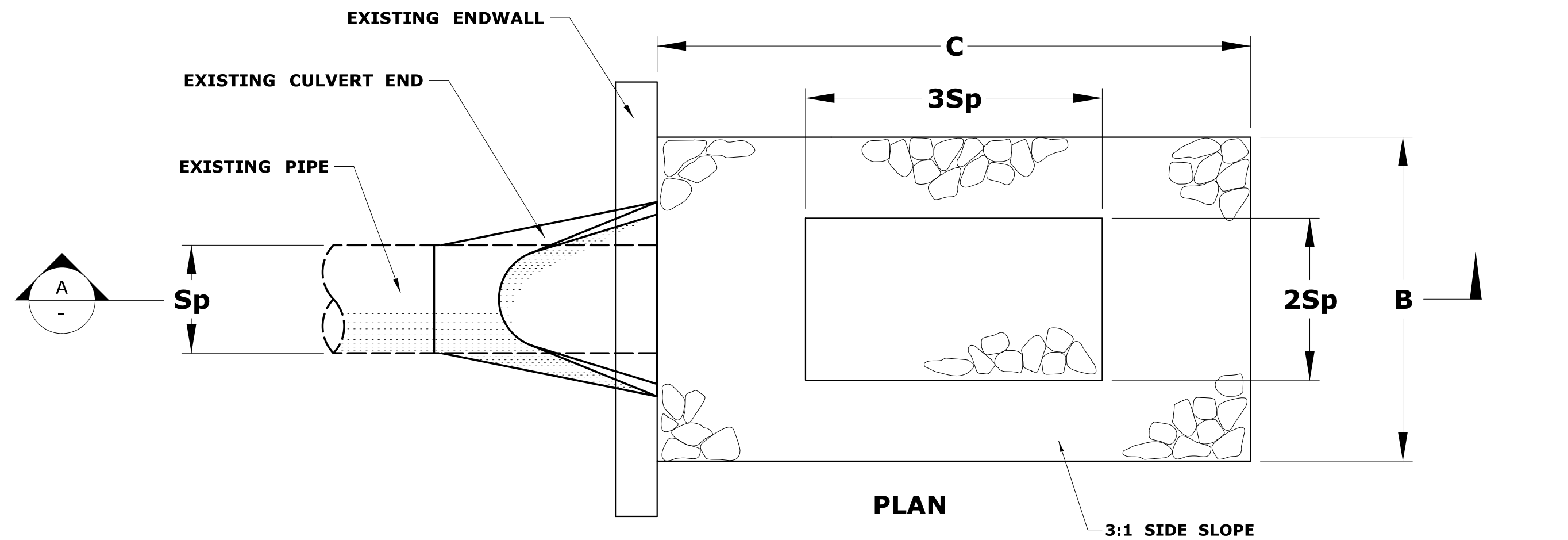
TYPICAL PAVEMENT TRANSITION AT RAMPS



WORK AREA ACCESS

NOTES:

1. WIDTHS AND LENGTHS OF WORK AREA ACCESS TO BE DETERMINED BY THE CONTRACTOR.
2. ANTI-TRACKING PAD SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CRITERIA SET FORTH UNDER THE CONTRACT ITEM *0100602 AND AS SHOWN ON THE DETAILS IN THE PLANS.



NOTES

1. SCOUR HOLE TYPE AND RIPRAP SIZE SHALL BE DETERMINED BY THE DISTRICT DRAINAGE ENGINEER.
2. GROUND ELEVATION (FINISHED GRADE) SHALL NOT BE INCREASED.
3. WORK SHALL BE IN ACCORDANCE WITH THE DEEP 2004 STORMWATER QUALITY MANUAL.
4. ALL TEMORARY WORK SHALL CONFORM WITH THE FLOOD MANAGEMENT GENERAL CERTIFICATION CONSTRAINTS.

LEGEND

Sp = Max. inside pipe span (non-circular sections)
or Inside pipe diameter (circular sections)

Rp = Max inside pipe rise (non-circular sections)
or Inside pipe diameter (circular sections)

d = 12" with Modified Riprap
18" with Intermediate Riprap
36" with Standard Riprap

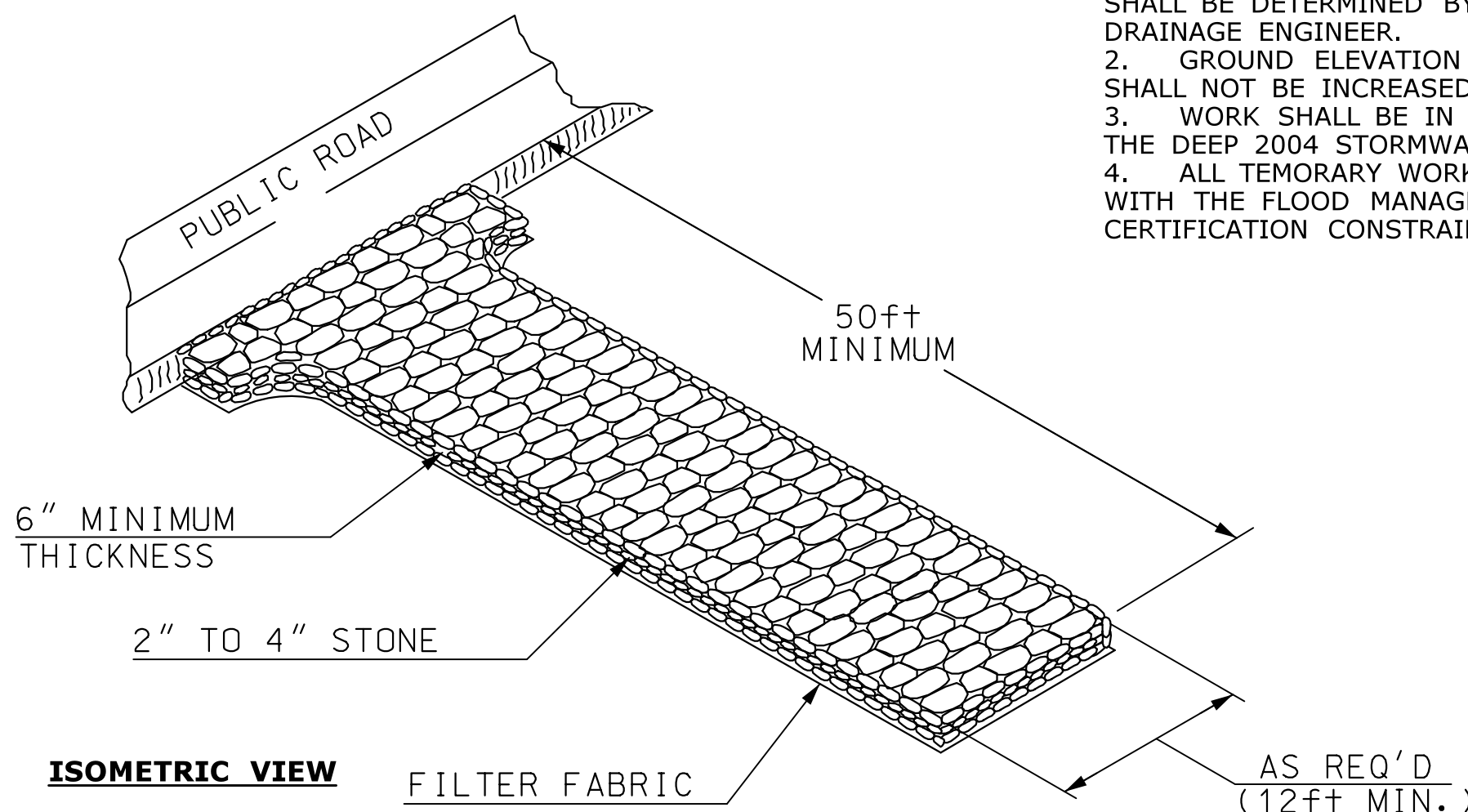
Scour Hole Type 1: F = 0.5 Rp

Scour Hole Type 2: F = Rp



C = 3Sp + 6F

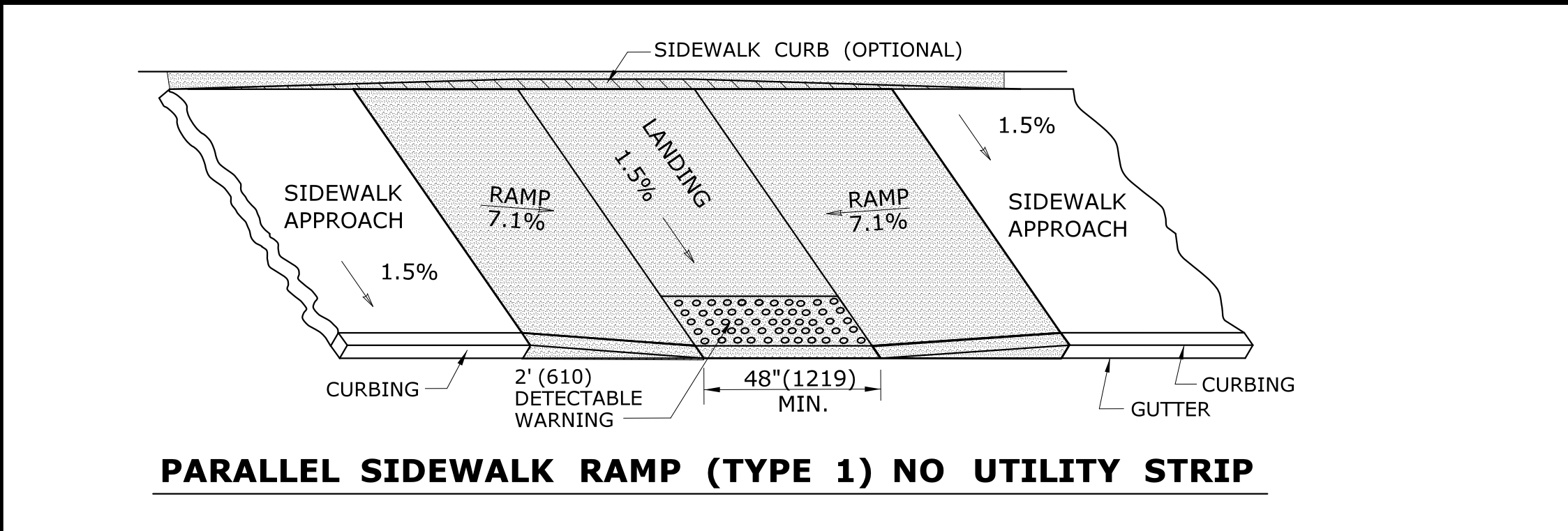
B = 2Sp + 6F

PREFORMED SCOUR HOLE

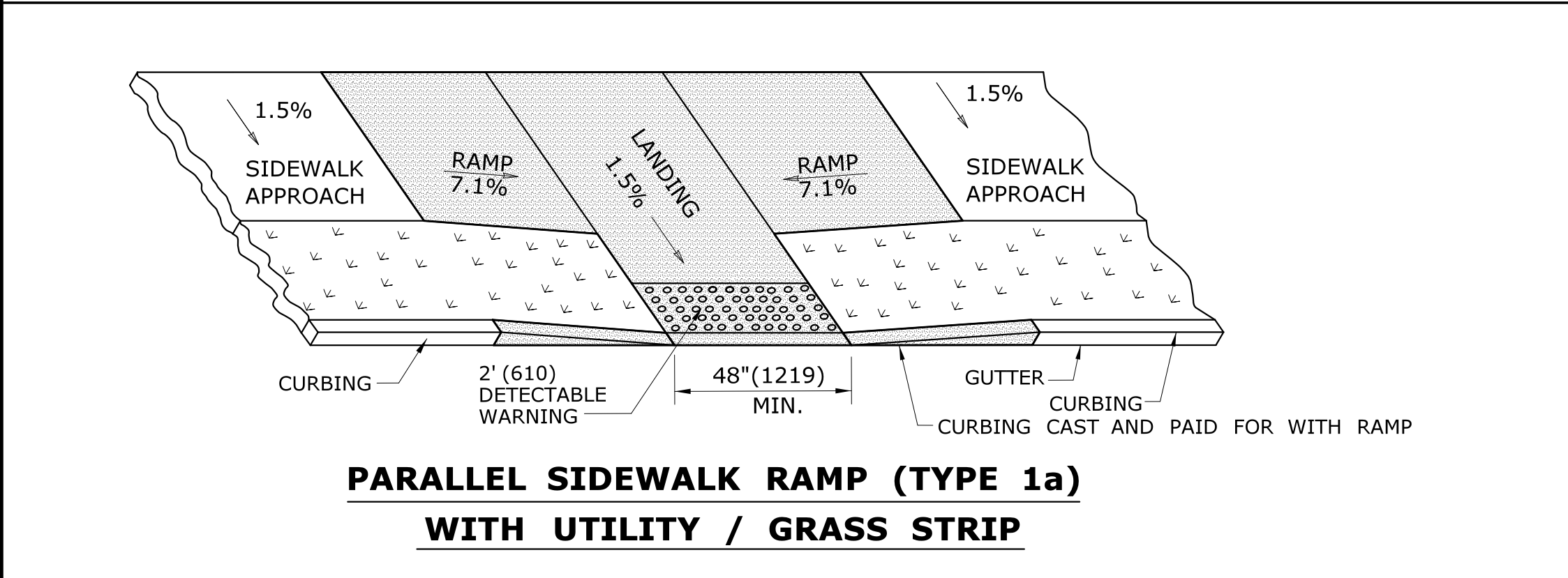


ANTI-TRACKING PAD

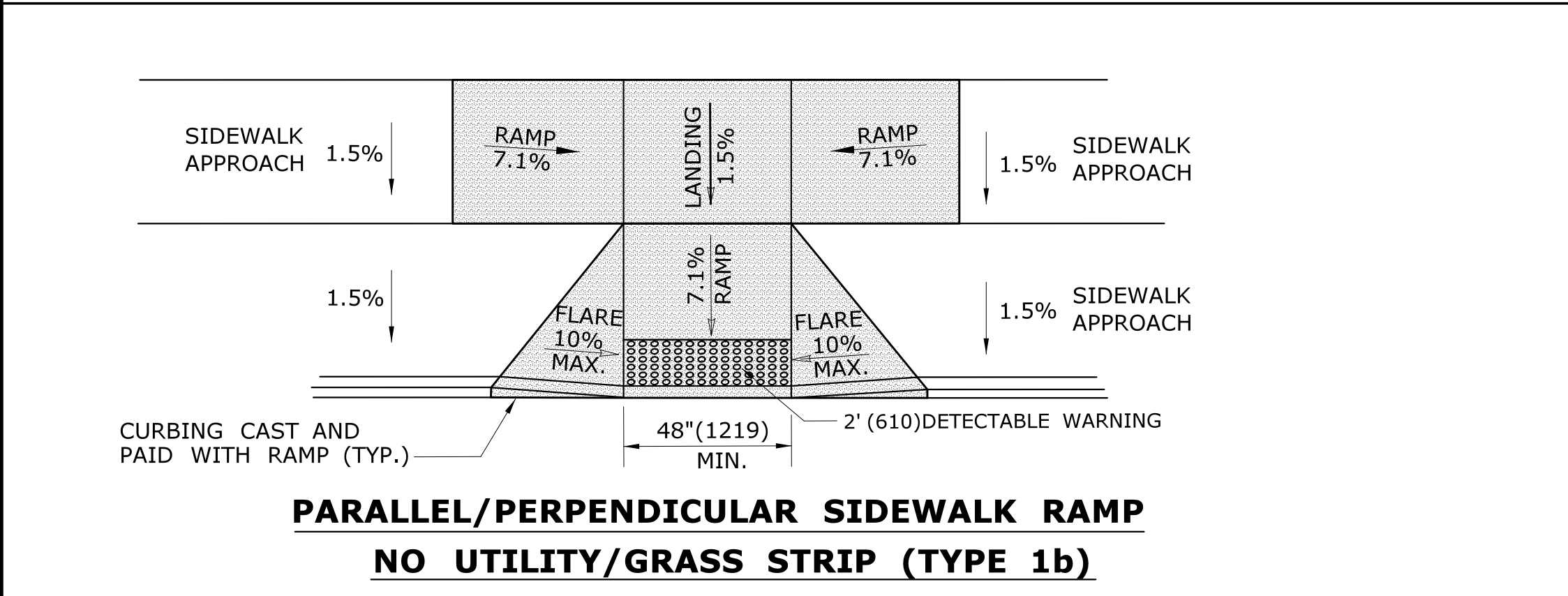
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				SCALE AS NOTED								
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 7/18/2016		Filename: ...\\158-211_Guide_12_RampTransition_TrackPad.dgn						



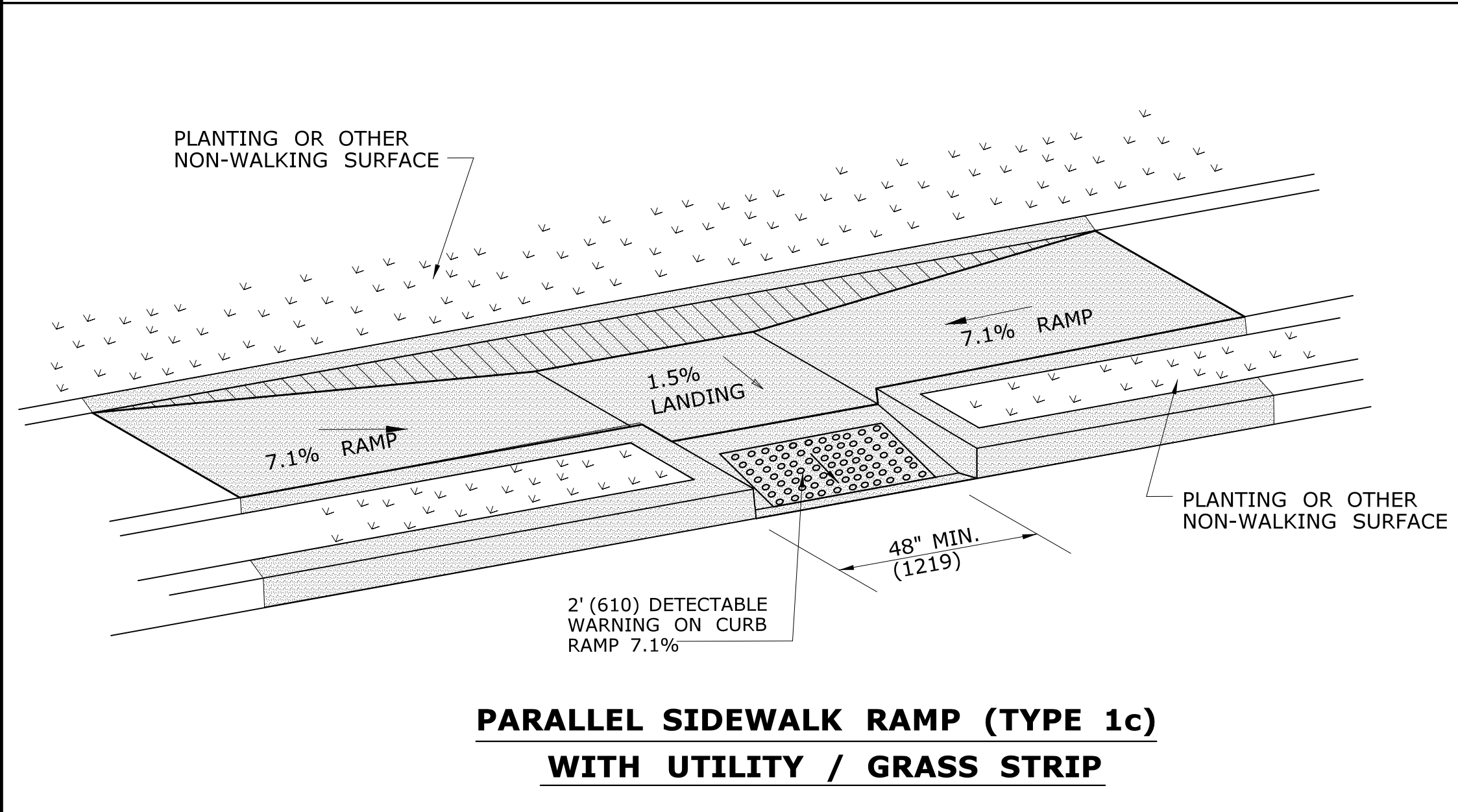
PARALLEL SIDEWALK RAMP (TYPE 1) NO UTILITY STRIP



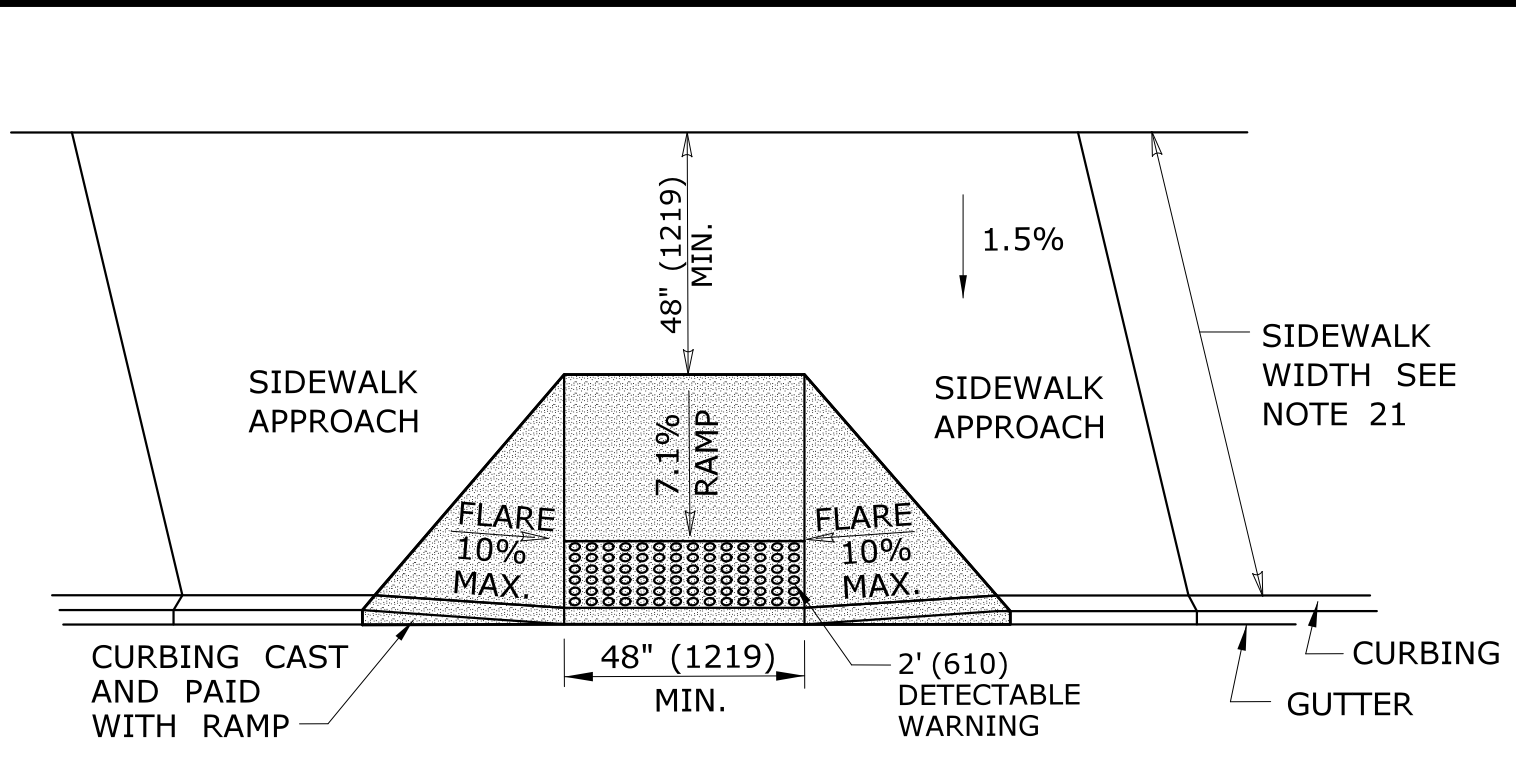
PARALLEL SIDEWALK RAMP (TYPE 1a)
WITH UTILITY / GRASS STRIP



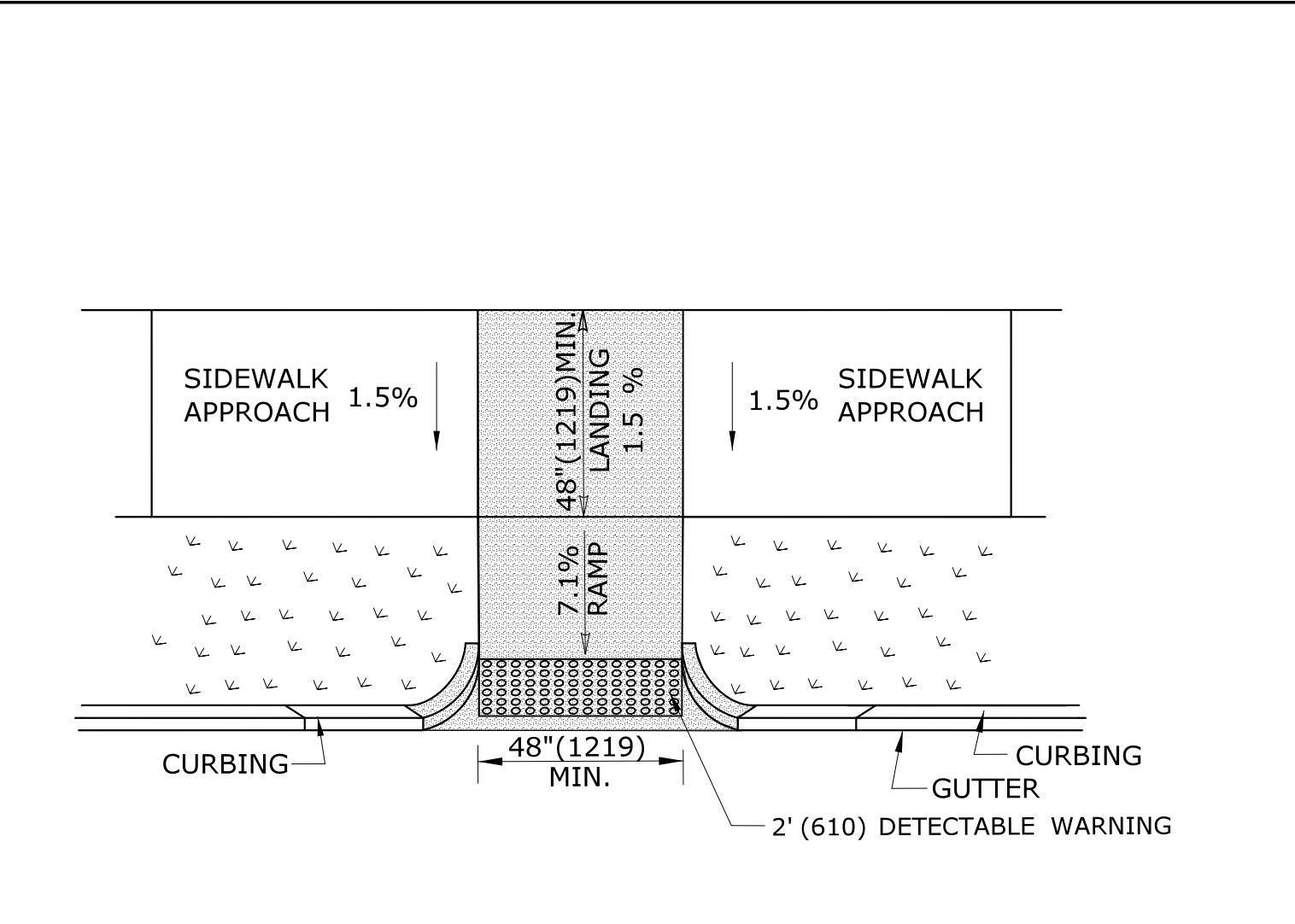
PARALLEL/PERPENDICULAR SIDEWALK RAMP
NO UTILITY/GRASS STRIP (TYPE 1b)



PARALLEL SIDEWALK RAMP (TYPE 1c)
WITH UTILITY / GRASS STRIP

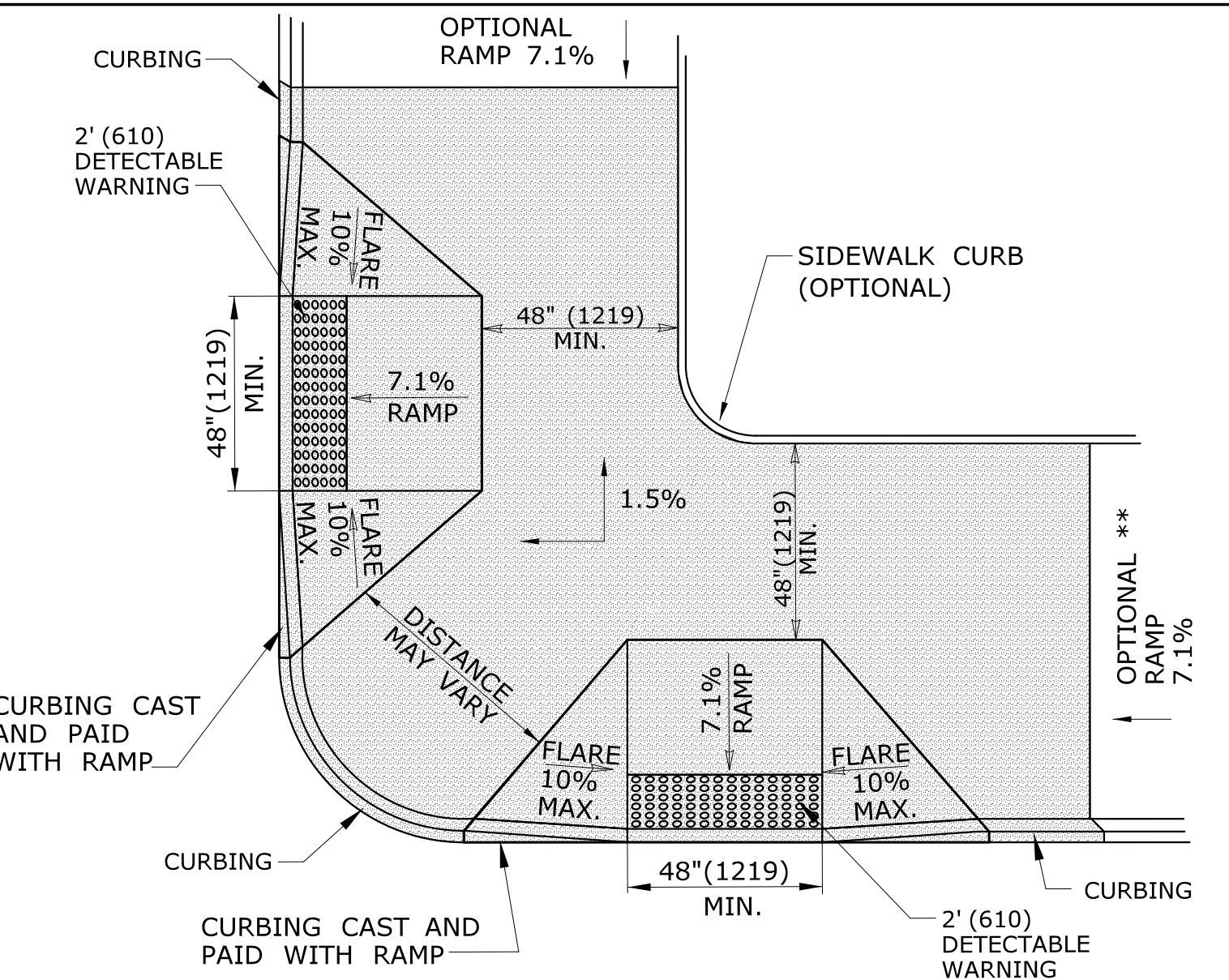


PERPENDICULAR SIDEWALK RAMP
W/ 48" (1219) MIN. BY PASS LANDING (TYPE 2)



PERPENDICULAR SIDEWALK RAMP
W/CURB RETURNS / UTILITY GRASS STRIP (TYPE 2a)

* OPTIONAL FLARE ONE SIDE OF RAMP



DUAL PERPENDICULAR
SIDEWALK RAMPS (TYPE 3)

SEE NOTES 19
* OPTIONAL CURB RETURN ON ONE SIDE OF RAMP
** SEE NOTE 23

GENERAL NOTES:

1. MAXIMUM SLOPES OF ADJOINING GUTTERS AND ROAD SURFACES IMMEDIATELY ADJACENT TO THE SIDEWALK RAMP SHOULD NOT EXCEED 5%. THE MAXIMUM GRADE DIFFERENCE BETWEEN THE GUTTER AND CURB RAMP SHALL NOT EXCEED 13%. SEE DETAIL 1 ON SHEET 4.
2. RAMP GRADE SHALL BE UNIFORM, FREE OF SAGS AND ABRUPT GRADE CHANGES. RUNNING SLOPES OF RAMPS SHALL NOT EXCEED 8.33% AND SHALL NOT EXCEED 15' (4.5m) WITHOUT PROVIDING A LANDING.
3. ALL RAMPS SHALL BE CONSTRUCTED OF CLASS "F" CONCRETE IN ACCORDANCE WITH CONNECTICUT STANDARD SPECIFICATIONS.
4. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP. THE SURFACE OF ALL SIDEWALK RAMPS SHALL BE STABLE, FIRM AND SLIP RESISTANT. SURFACE DISCONTINUITIES SHALL NOT EXCEED 1/2" (13) MAX. VERTICAL DISCONTINUITIES BETWEEN 1/4" (6.4) AND 1/2" (13) MAX. SHALL BE BEVELED 1:2 MINIMUM APPLIED ACROSS THE ENTIRE LEVEL CHANGE.
5. DIAGONAL SIDEWALK RAMPS AT MARKED CROSSINGS SHALL BE WHOLLY CONTAINED WITHIN THE MARKINGS, EXCLUDING ANY FLARED SIDES. DIAGONAL AND PERPENDICULAR RAMPS SHALL HAVE THE RAMP CUT PERPENDICULAR TO THE TANGENT OF THE CURB RADIUS FOR THE DESIGNATED ACCESSIBLE ROUTE. BOTH LONGITUDINAL SIDES OF THE RAMP SHOULD BE THE SAME LENGTH. SKEWED RAMPS SHOULD BE AVOIDED. FLARES ARE NOT CONSIDERED PART OF PEDESTRIAN ACCESS ROUTE. DIAGONAL RAMPS SHOULD NOT BE INSTALLED WHERE CURB RADII IS LESS THAN 20'(6096).
6. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT. 8.3% MAXIMUM SLOPE MAY NOT BE ACHIEVABLE DUE TO EXISTING SIDEWALK GRADE. IN RECOGNITION OF THIS, A LIMIT OF 15' (4572) FOR REMOVAL SHALL BE USED UNLESS OTHERWISE SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER. SAW CUT REQUIRED FOR DUMMY JOINTS SHALL BE INCLUDED IN THE COST OF "CONCRETE SIDEWALK RAMP" OR "CONCRETE SIDEWALK".
7. EXPANSION JOINTS IN CONCRETE SHALL MATCH THOSE IN ADJACENT SIDEWALKS BUT IN NO CASE SHALL THE SPACING BETWEEN EXPANSION JOINTS EXCEED 12' (3658) UNLESS OTHERWISE NOTED.
8. CONCRETE SIDEWALK RAMPS, SHALL BE PAID FOR UNDER THE ITEM "CONCRETE SIDEWALK RAMP", AS DEFINED BY THE CONSTRUCTION LIMITS ON THE PLANS AND SHALL BE FIELD VERIFIED.
9. SIDEWALK RAMPS SHALL BE CONSTRUCTED WITH THE TOE AT THE GUTTER CAST INTEGRALLY WITH RAMP UNLESS DIRECTED OTHERWISE BY THE ENGINEER (SEE TYPICAL SECTION ON SHEET 3). CURB REMOVAL AND CAST IN PLACE CURBING REQUIRED FOR THE RAMP, SHALL BE INCLUDED WITH PAY ITEM "CONCRETE SIDEWALK RAMP". CURBING OUTSIDE LIMITS OF RAMP OR LANDING SHOWN ON SHEET 3 SHALL BE CONSTRUCTED AND PAID FOR IN ACCORDANCE WITH CONNECTICUT STANDARD SPECIFICATIONS.
10. PREFERRED LOCATION TO INSTALL DETECTABLE WARNING STRIP SHALL BE 6" (152) FROM THE EDGE OF ROAD ALONG THE FULL WIDTH OF THE RAMP. FOR ALTERNATE LOCATIONS, REFER TO DETECTABLE WARNING PLACEMENT DETAILS ON SHEET 4.
11. TO PERMIT WHEELCHAIR WHEELS TO ROLL BETWEEN DOMES, ALIGN DOMES ON A SQUARE GRID IN THE DIRECTION OF RUNNING SLOPE (PERPENDICULAR TO CURB OR SLOPE BREAK). THE TRANSITION FROM RAMP TO GUTTER SHALL BE FLUSH WITHOUT A LIP.
12. WHERE COMMERCIAL DRIVEWAYS ARE PROVIDED WITH TRAFFIC SIGNALS AND THE SIDEWALK IS CONTINUOUS THROUGH DRIVEWAY, DETECTABLE WARNINGS ARE REQUIRED AT THE JUNCTION BETWEEN THE PEDESTRIAN ROUTE AND DRIVEWAY.
13. CONSTRUCT A SIDEWALK CURB WHEN THERE IS INSUFFICIENT BUFFER AVAILABLE TO GRADE OR WHEN CALLED FOR IN PLANS. PAID FOR WITH SIDEWALK RAMP WHEN REQUIRED FOR RAMP.
14. THE TOP AND BOTTOM OF RAMPS SHOULD BE PROVIDED WITH A 4'x 4' (1219 x 1219) MINIMUM LEVEL LANDING AREA WITH A CROSS SLOPE LESS THAN OR EQUAL TO 2% IN ANY DIRECTION.
15. UTILITY POLES, LUMINAIRE, PEDESTRIAN OR SIGNAL POLES, GRATES, ACCESS COVERS, AND OTHER APPURTENANCES SHALL NOT BE LOCATED ON RAMPS, LANDINGS, BLENDED TRANSITIONS, AND @ GUTTERS WITHIN THE PEDESTRIAN ACCESS ROUTE.
16. APPROACH SIDEWALK WIDTHS, GRASS STRIP OR UTILITY STRIP WIDTHS MAY VARY.
17. APPROACH SIDEWALK AND LANDING CROSS SLOPE SHALL NOT EXCEED 2%.
18. THE RUNNING OR CROSS SLOPES ON LANDINGS AT MID BLOCK CROSSING MAY BE WARPED TO MEET STREET OR HIGHWAY GRADE.
19. FOR PERPENDICULAR CURB RAMPS A MIN. 4'(1.2m) x 4'(1.2m) LEVEL LANDING SHALL BE PROVIDED AT THE TOP OF CURB RAMP. WHERE THE LEVEL LANDING IS RESTRICTED AT THE BACK OF SIDEWALK THE LEVEL LANDING SHALL BE 4'(1.2m) x 5'(1.5m) WITH THE 5'(1.5m) DIMENSION PROVIDED IN THE DIRECTION OF THE RAMP RUN.
20. FOR PARALLEL CURB RAMPS, A MIN. 4'(1.2m) x 4'(1.2m) LEVEL LANDING SHALL BE PROVIDED AT THE BOTTOM OF CURB RAMP. IF THE LEVEL LANDING IS RESTRICTED ON 2 OR MORE SIDES, THE LEVEL LANDING SHALL BE 4'(1.2m) x 5'(1.5m) WITH THE 5'(1.5m) DIMENSION PROVIDED IN THE DIRECTION OF THE PEDESTRIAN STREET CROSSING.
21. WHEN WIDTH OF SIDEWALK IS ≥48" AND A PERPENDICULAR SIDEWALK RAMP IS INSTALLED, THE FLARED SIDES SHALL BE 10% MAX. IF WIDTH OF SIDEWALK IS <48" THE FLARED SIDES MUST NOT EXCEED 8.33% (12:1).
22. SHADED AREAS ARE TYPICAL PAY LIMITS FOR CONCRETE SIDEWALK RAMP BUT, MAY VARY AS DIRECTED BY THE ENGINEER.
23. OPTIONAL RAMP, WHEN REQUIRED, SHALL BE PAID FOR AS PART OF CONCRETE SIDEWALK RAMP.


DUAL PERPENDICULAR SIDEWALK RAMPS (TYPE 3a)
WITH UTILITY / GRASS STRIP

SEE NOTE 20

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.


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DESIGNER/DRAFTER: MGB/EMK
CHECKED BY: LLF
SCALE AS NOTED

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DEPARTMENT OF TRANSPORTATION

Filename: ...\\158-211_Guide-13 - Sidewalk Ramp 1.dgn

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BLOCK:
OFFICE OF ENGINEERING

APPROVED BY:


PROJECT TITLE:
**ROUTE 15 SAFETY IMPROVEMENTS,
RESURFACING, ENHANCEMENTS,
AND BRIDGE IMPROVEMENTS**

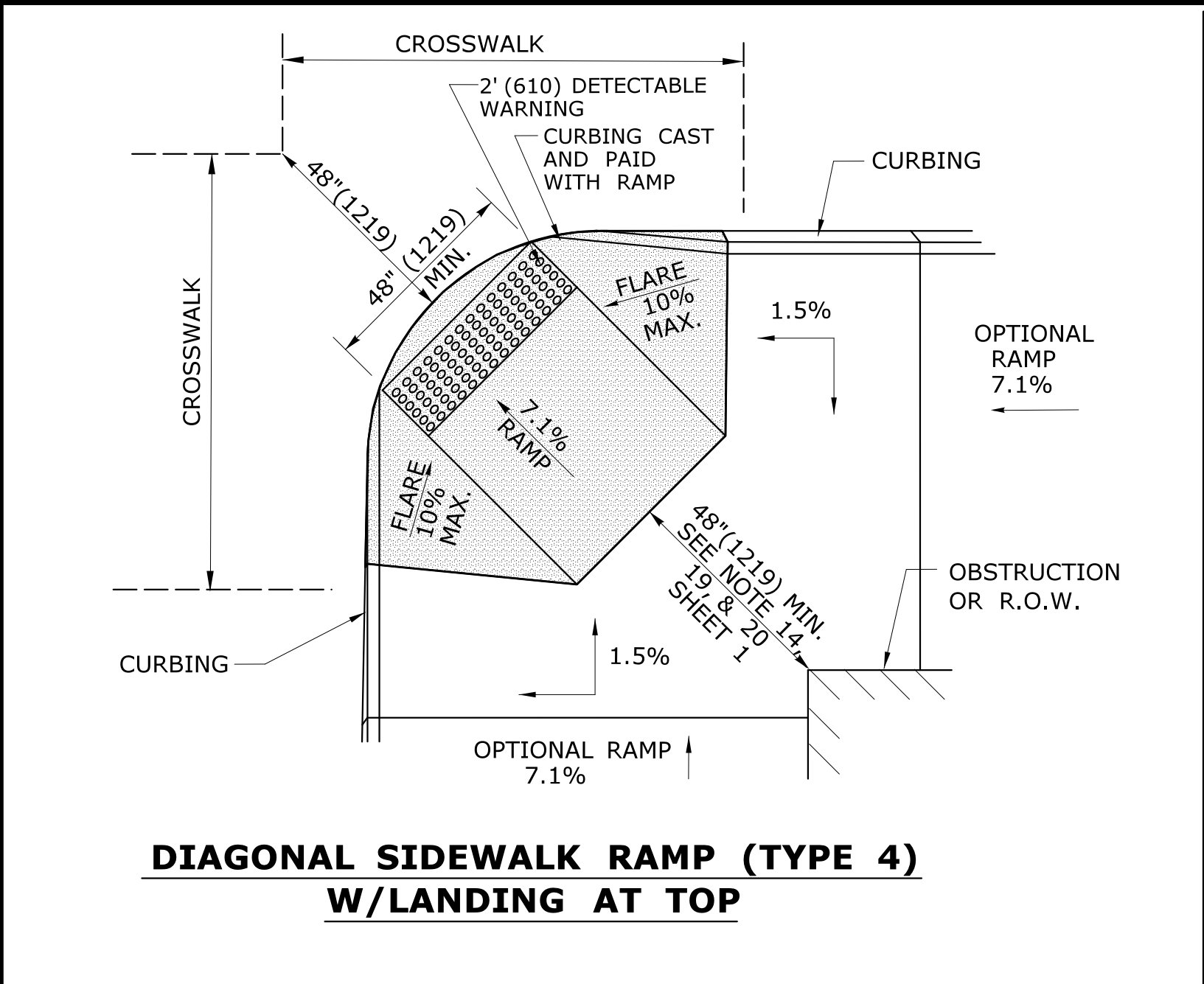
TOWN:
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FAIRFIELD**

DRAWING TITLE:
**SIDEWALK RAMPS
SHEET 1**

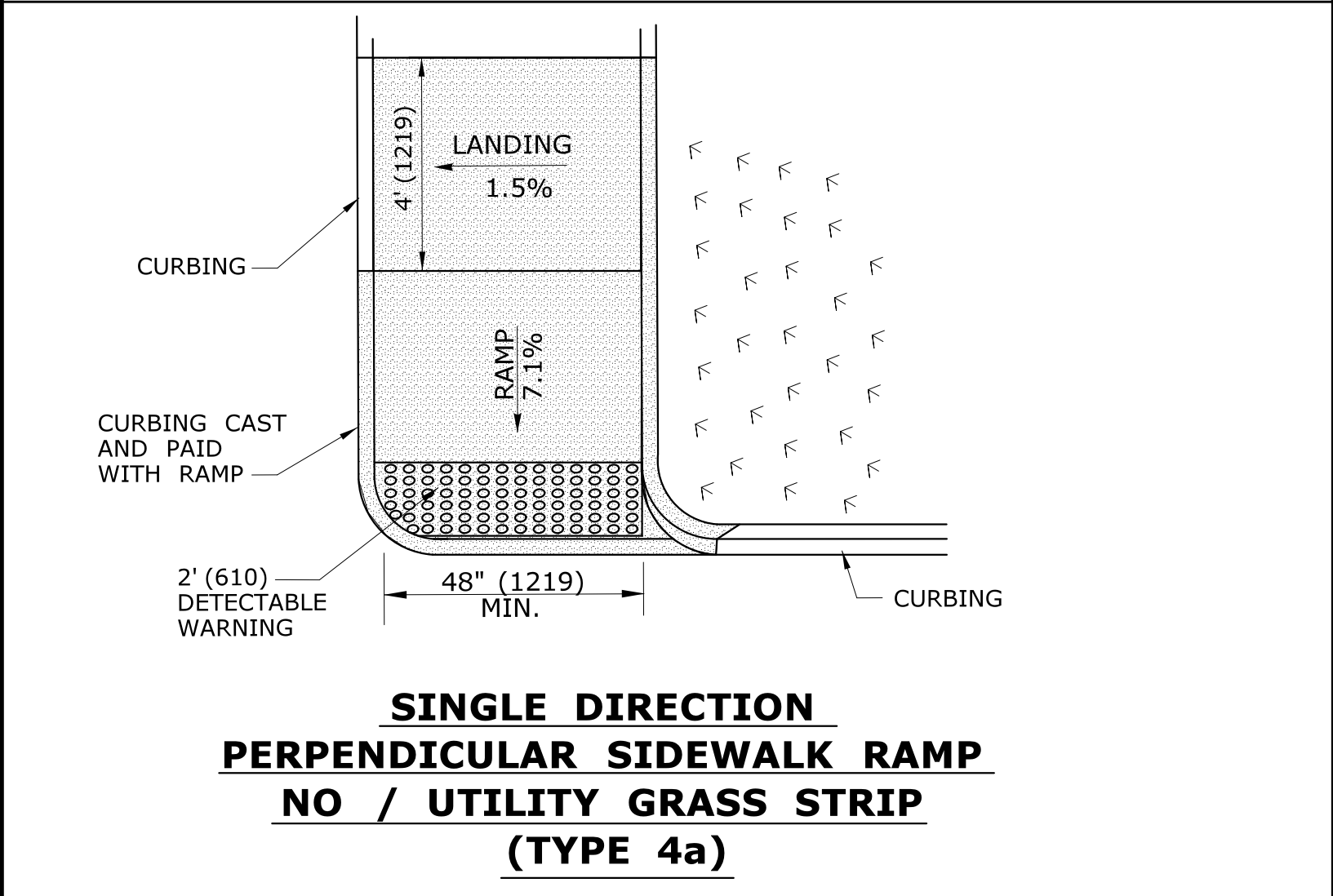
PROJECT NO.
158-211

DRAWING NO.
GDS-13

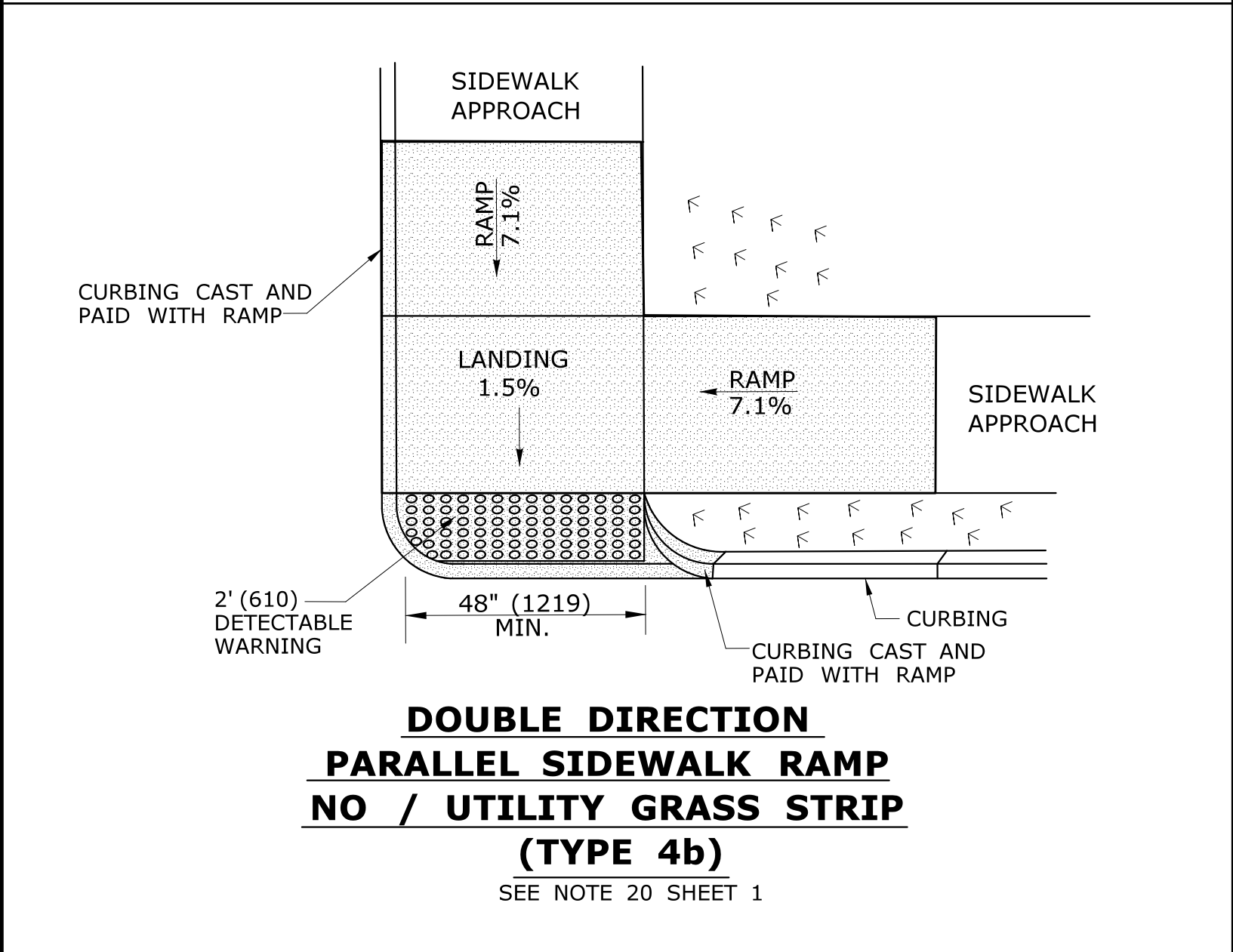
SHEET NO.
01.03.20



**DIAGONAL SIDEWALK RAMP (TYPE 4)
W/LANDING AT TOP**

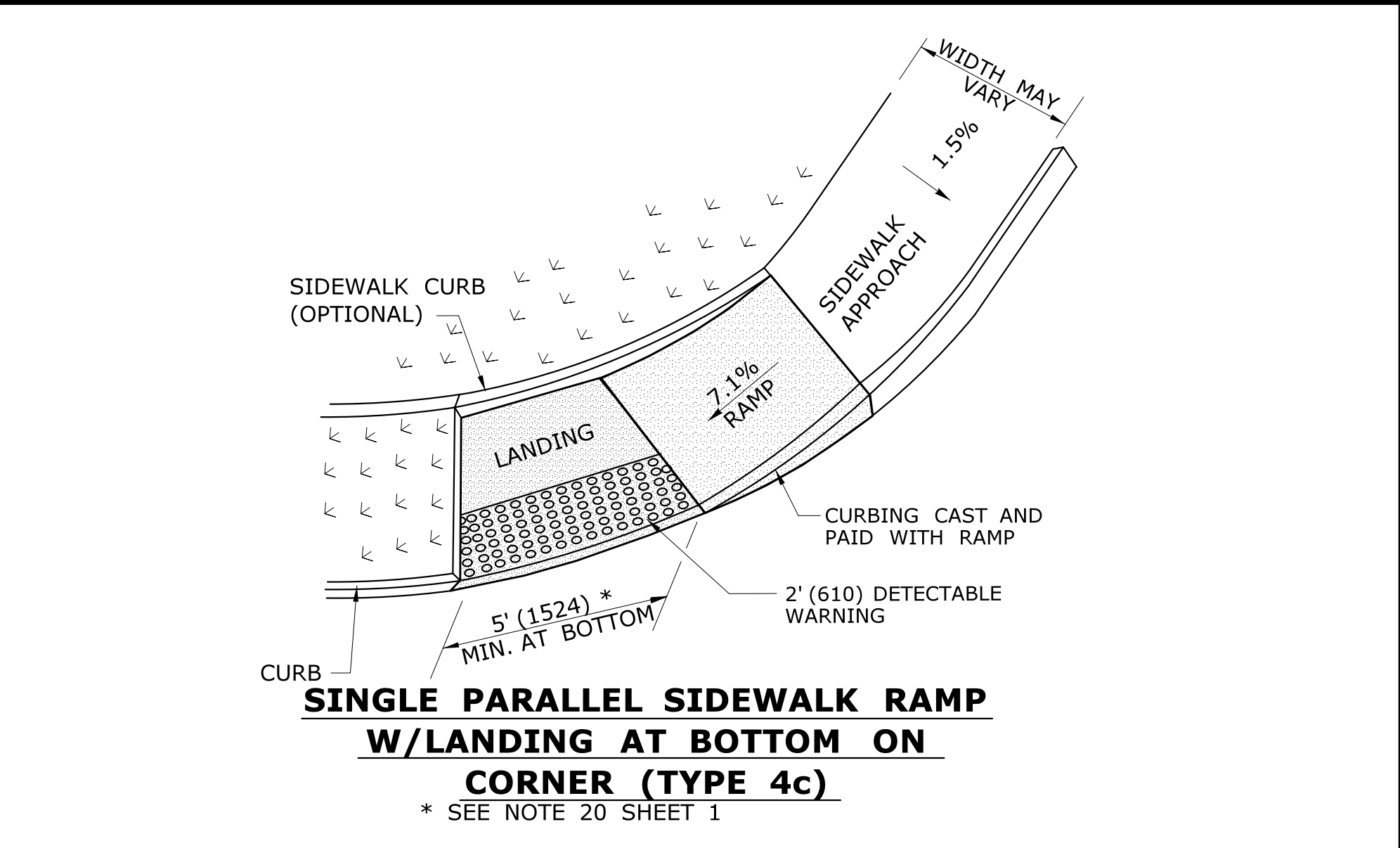


**SINGLE DIRECTION
PERPENDICULAR SIDEWALK RAMP
NO / UTILITY GRASS STRIP
(TYPE 4a)**



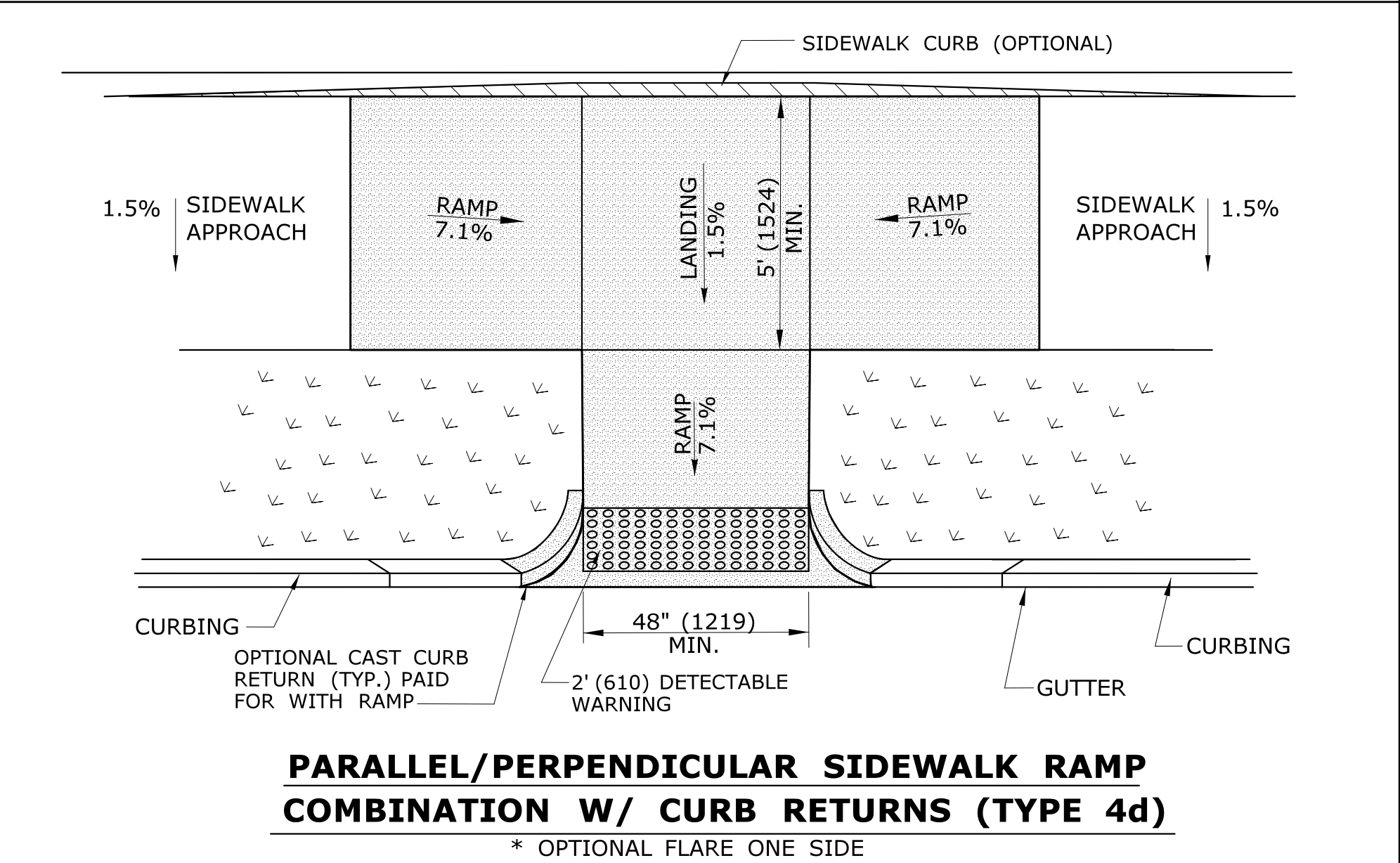
**DOUBLE DIRECTION
PARALLEL SIDEWALK RAMP
NO / UTILITY GRASS STRIP
(TYPE 4b)**

SEE NOTE 20 SHEET 1



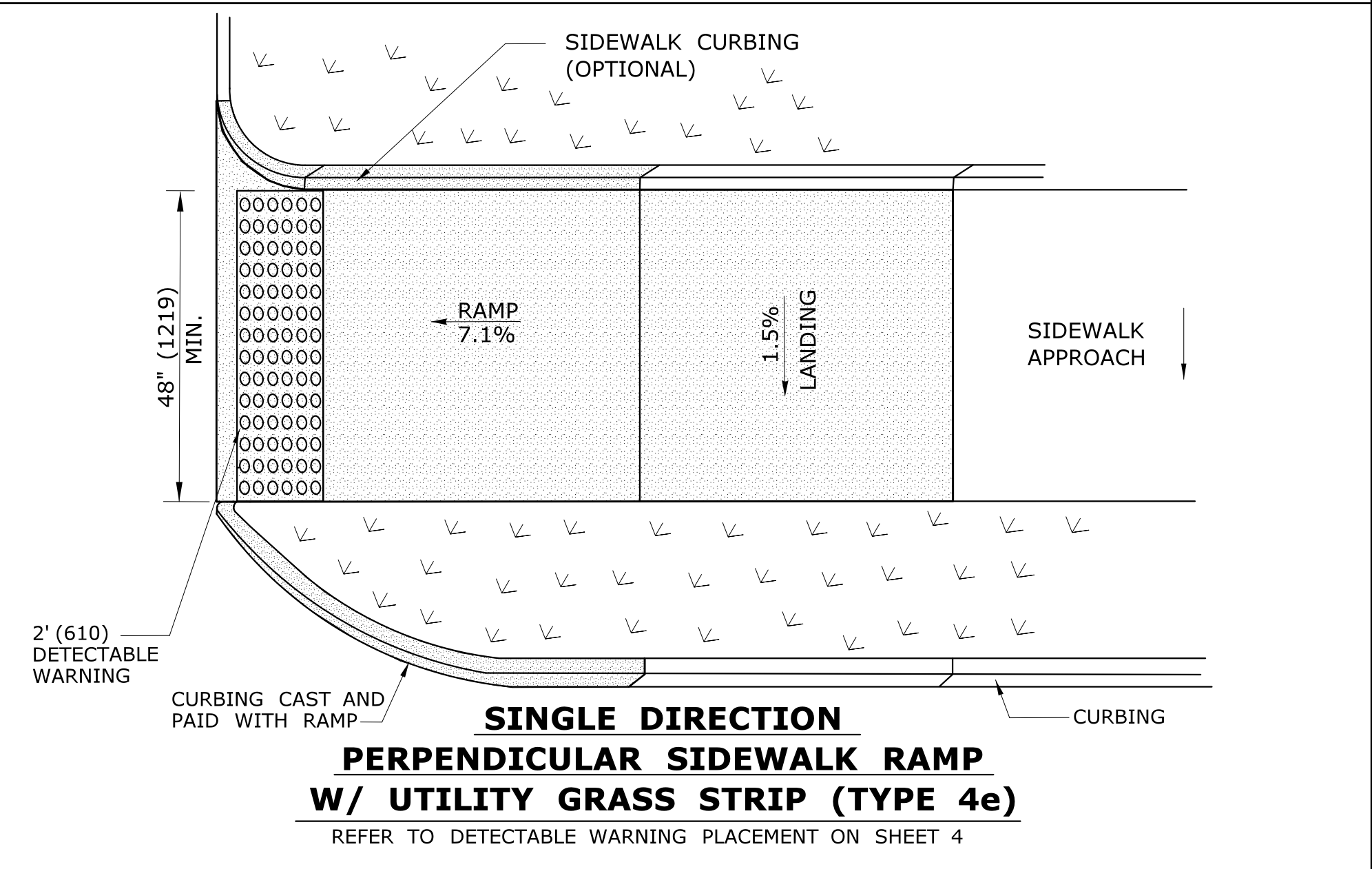
**SINGLE PARALLEL SIDEWALK RAMP
W/LANDING AT BOTTOM ON
CORNER (TYPE 4c)**

* SEE NOTE 20 SHEET 1



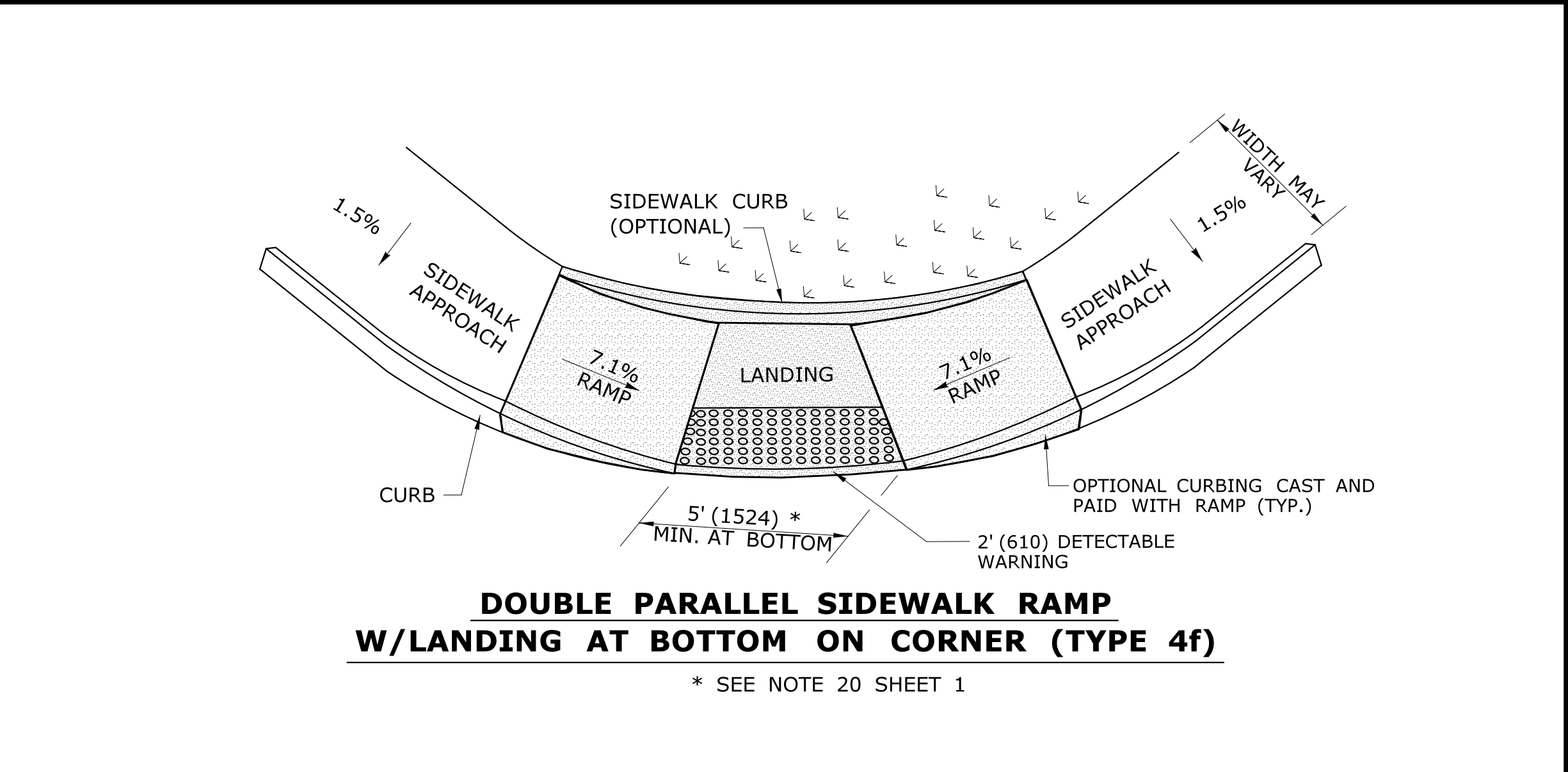
**PARALLEL/PERPENDICULAR SIDEWALK RAMP
COMBINATION W/ CURB RETURNS (TYPE 4d)**

* OPTIONAL FLARE ONE SIDE



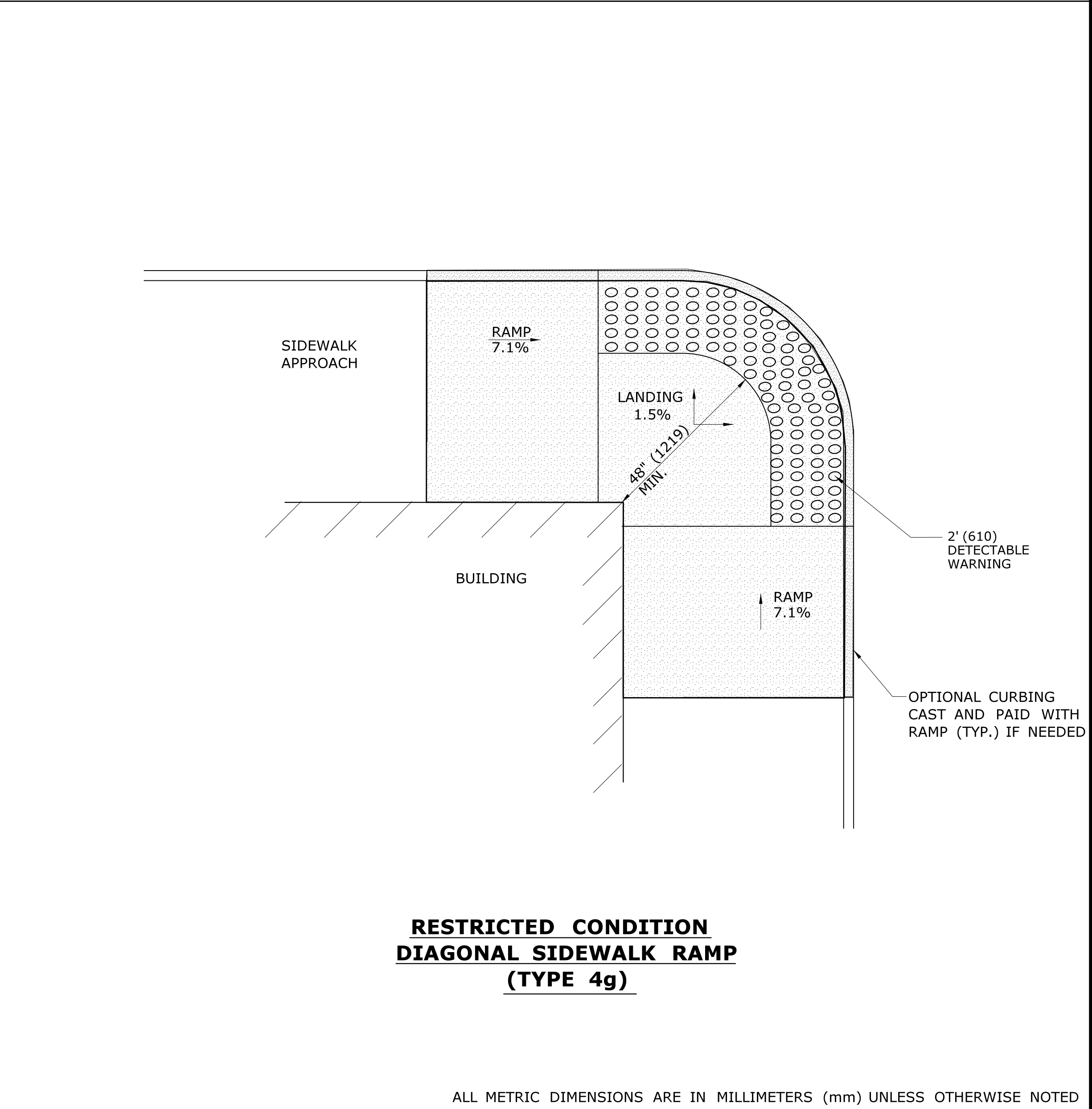
**SINGLE DIRECTION
PERPENDICULAR SIDEWALK RAMP
W/ UTILITY GRASS STRIP (TYPE 4e)**

REFER TO DETECTABLE WARNING PLACEMENT ON SHEET 4



**DOUBLE PARALLEL SIDEWALK RAMP
W/LANDING AT BOTTOM ON CORNER (TYPE 4f)**

* SEE NOTE 20 SHEET 1

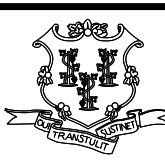


**RESTRICTED CONDITION
DIAGONAL SIDEWALK RAMP
(TYPE 4g)**

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

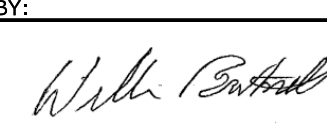
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DESIGNER/DRAFTER: MGB/EMK
CHECKED BY: LLF
SCALE AS NOTED

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PROJECT TITLE:
**ROUTE 15 SAFETY IMPROVEMENTS,
RESURFACING, ENHANCEMENTS,
AND BRIDGE IMPROVEMENTS**

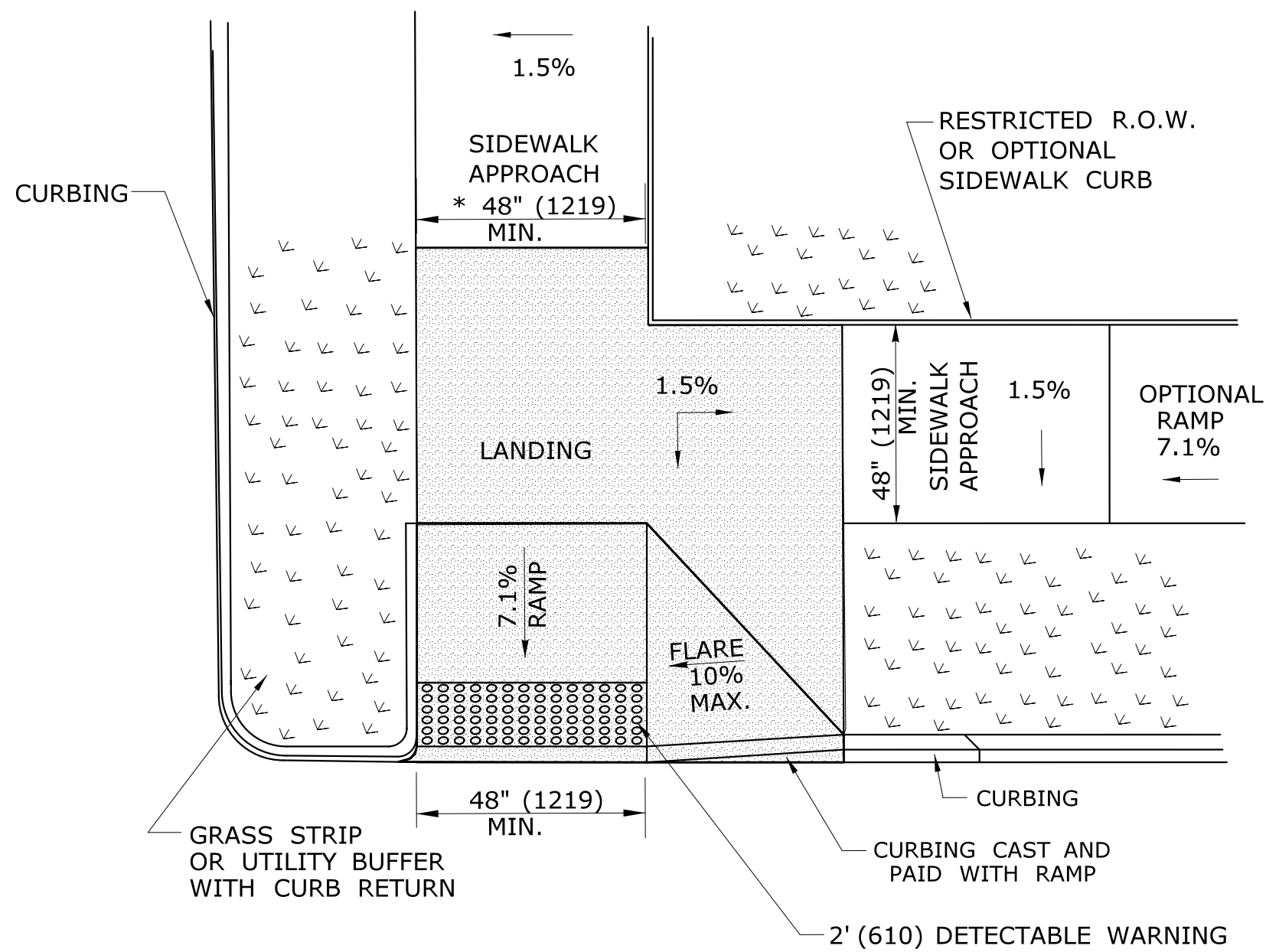
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**SIDEWALK RAMP
SHEET 2**

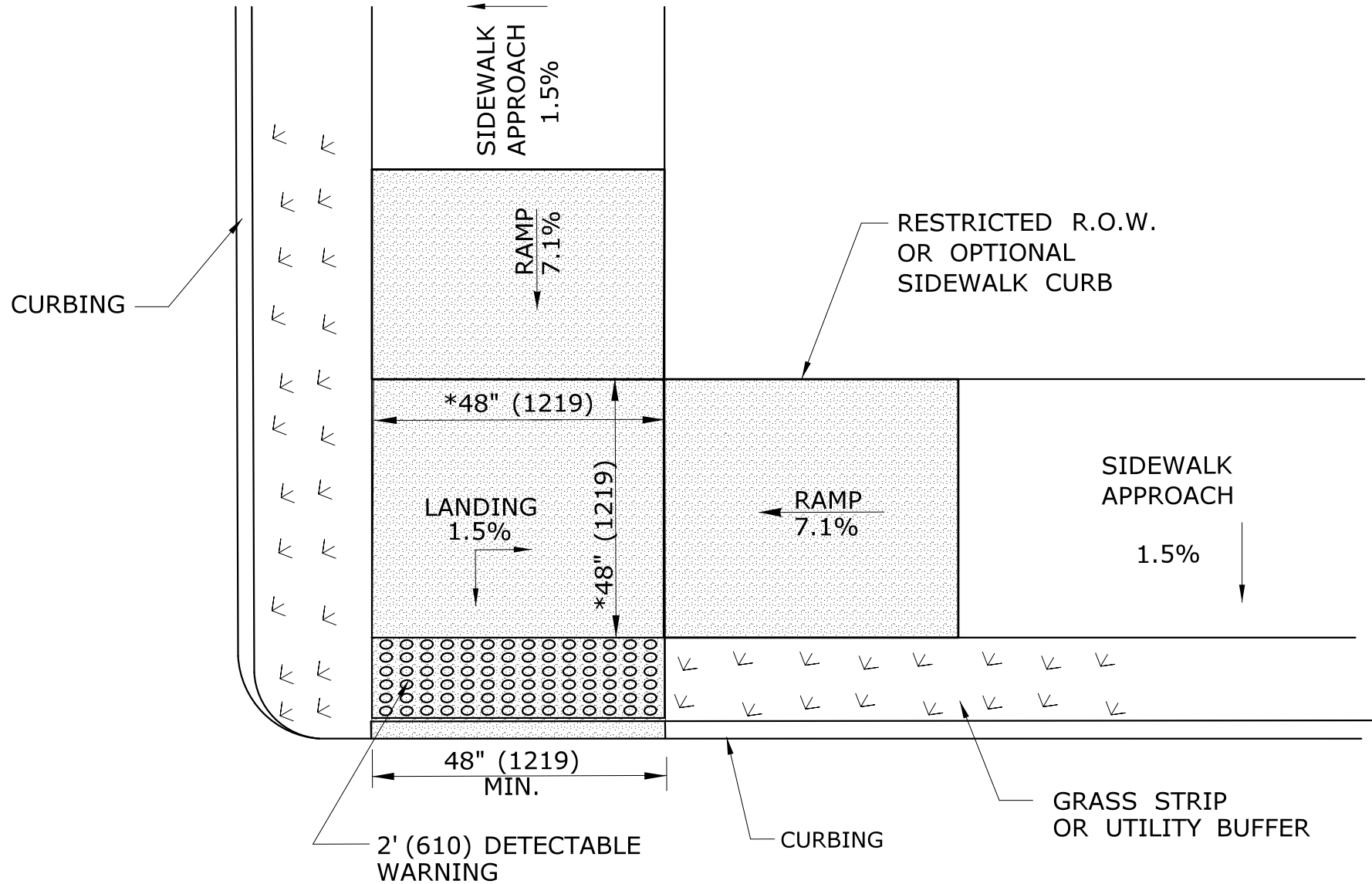
PROJECT NO.
158-211

DRAWING NO.
GDS-14

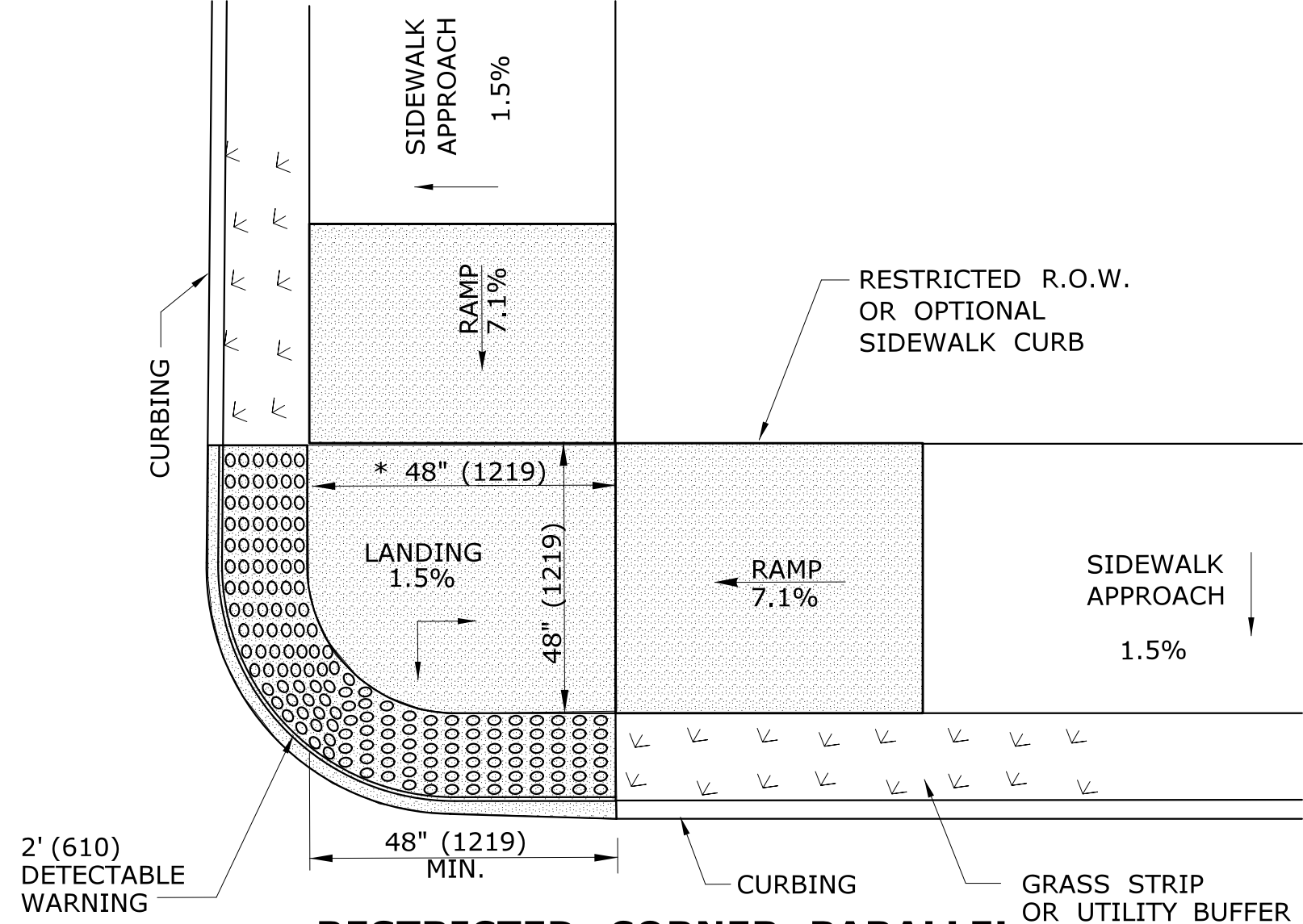
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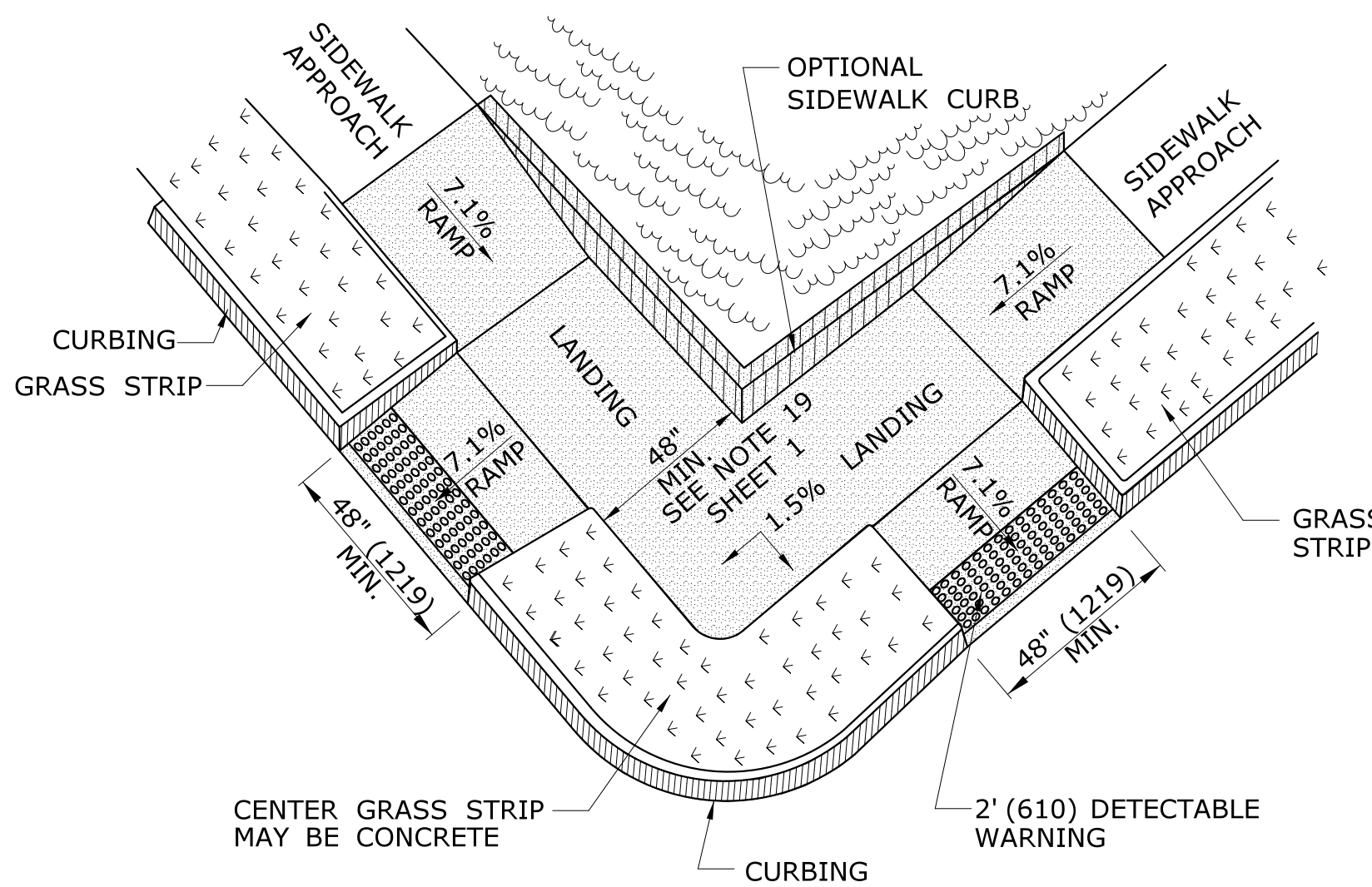
**RESTRICTED CORNER PERPENDICULAR SIDEWALK RAMP
(TYPE 5)**
* SEE NOTE 19 SHEET 1



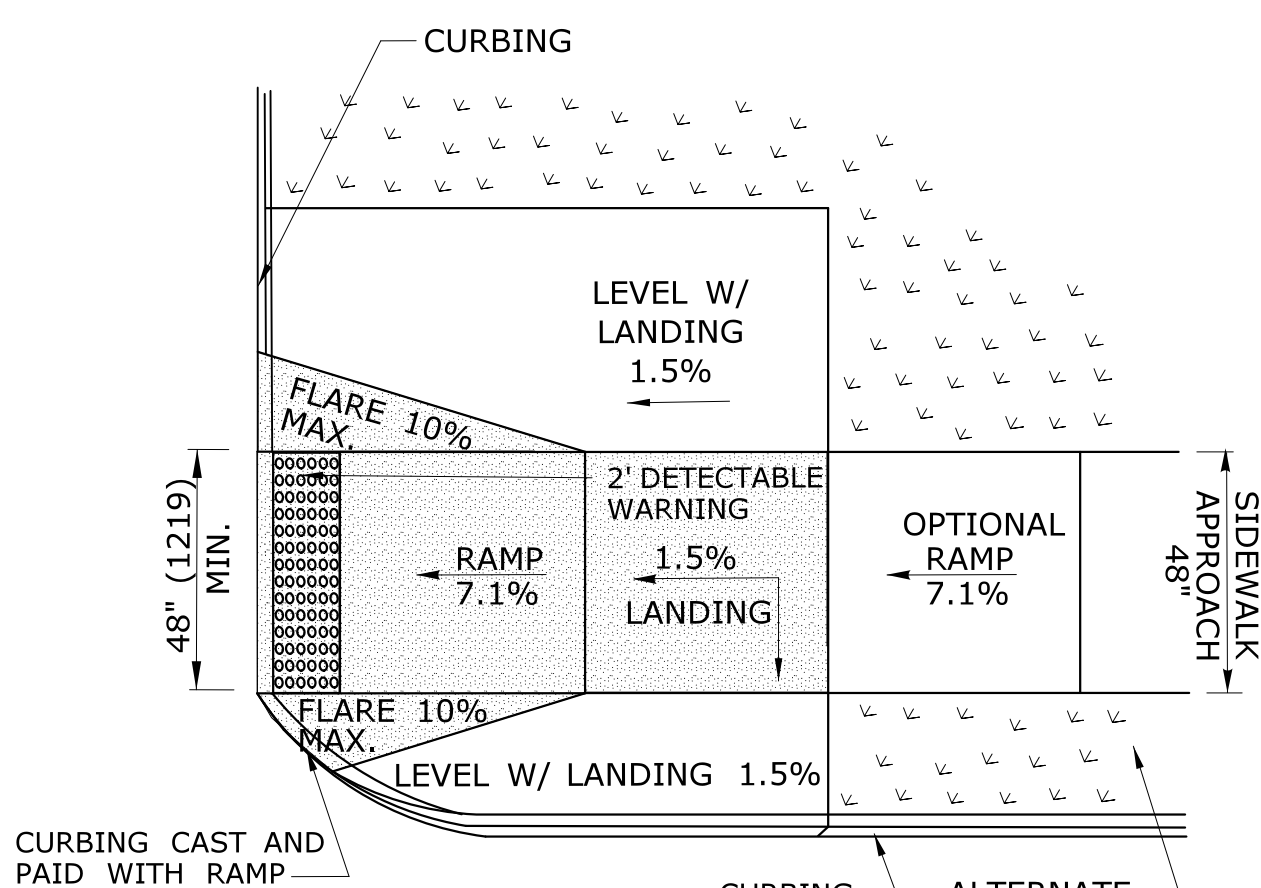
**RESTRICTED CORNER PARALLEL DOUBLE SIDEWALK RAMP
W/CENTER LANDING AND UTILITY GRASS STRIP
(TYPE 5a)**
* SEE NOTE 20 SHEET 1



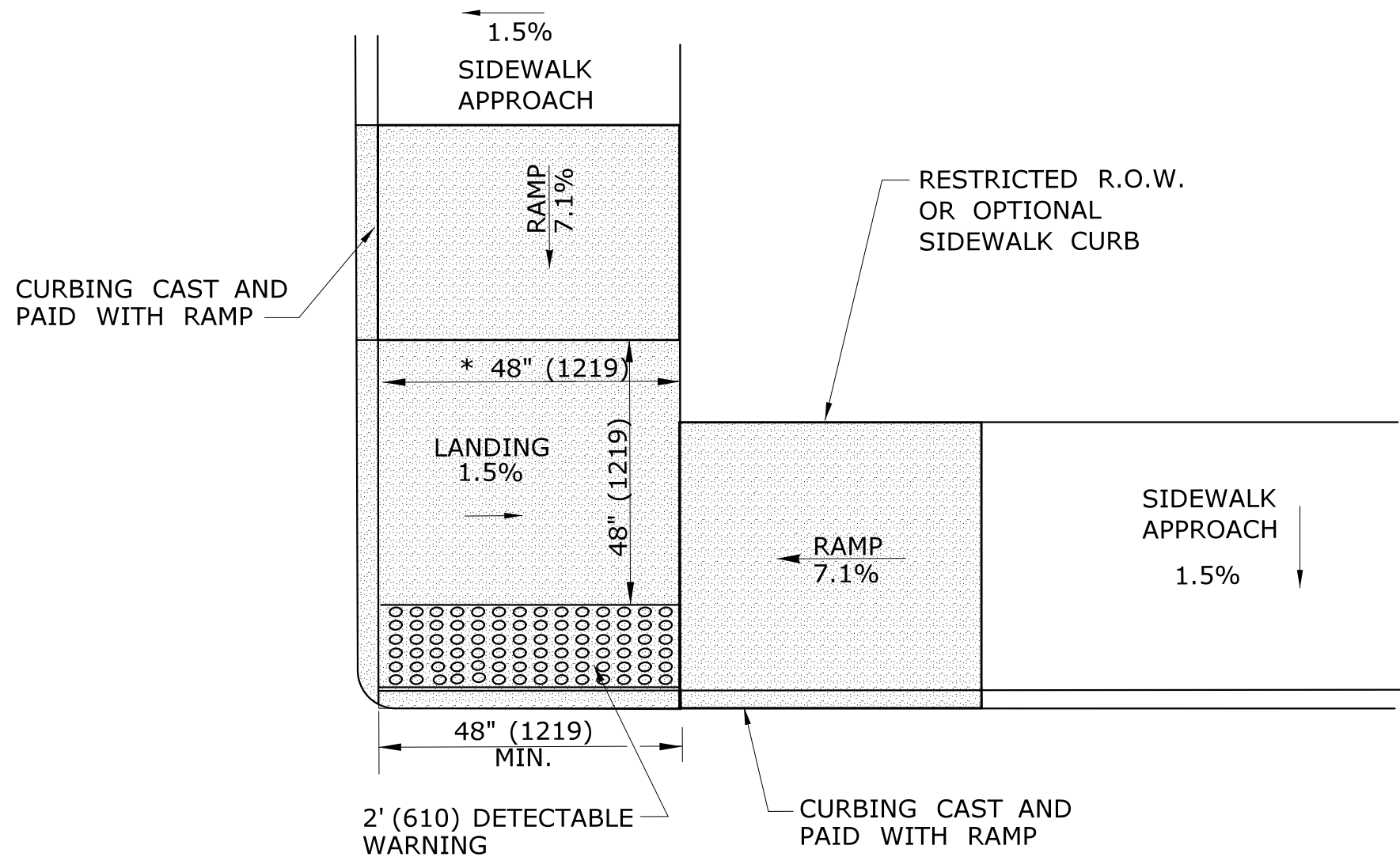
**RESTRICTED CORNER PARALLEL
SIDEWALK RAMP W/ CENTER LANDING
TWO DIRECTION AND UTILITY GRASS STRIP
(TYPE 5b)**
* SEE NOTE 20 SHEET 1



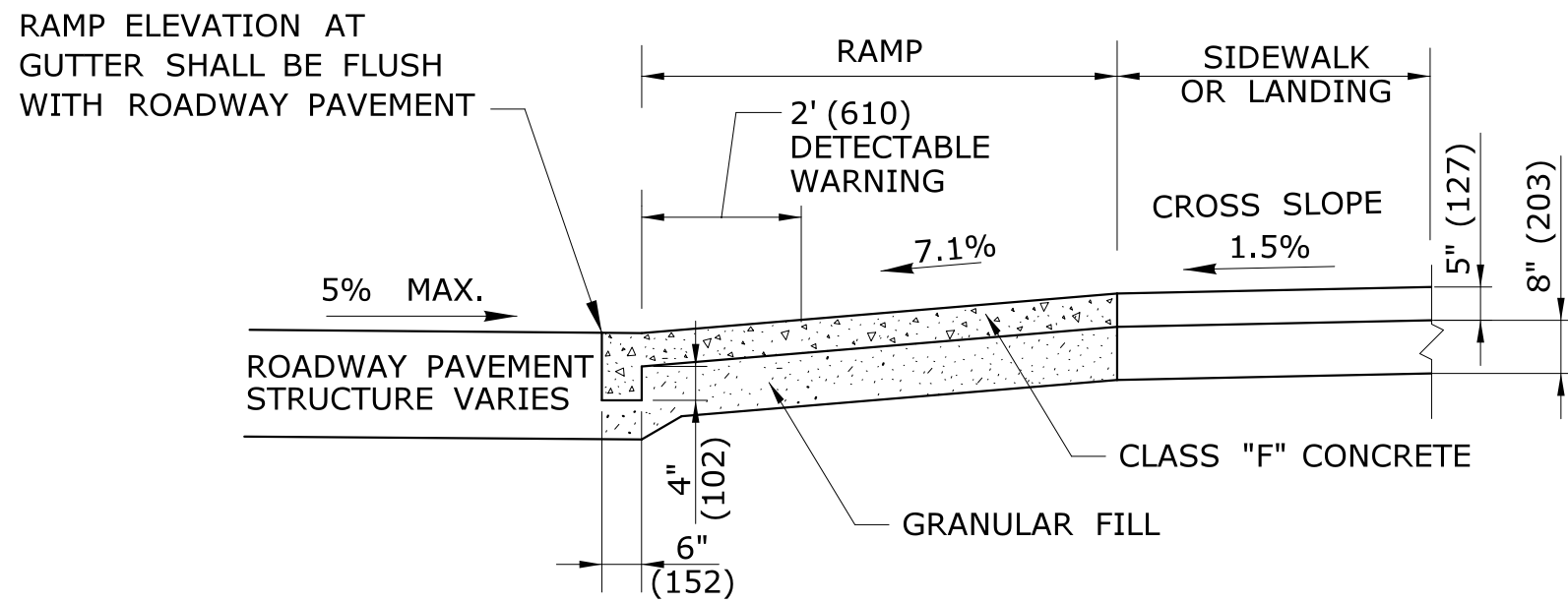
COMBINATION SIDEWALK RAMP (TYPE 6)
* SEE NOTE 19 SHEET 1



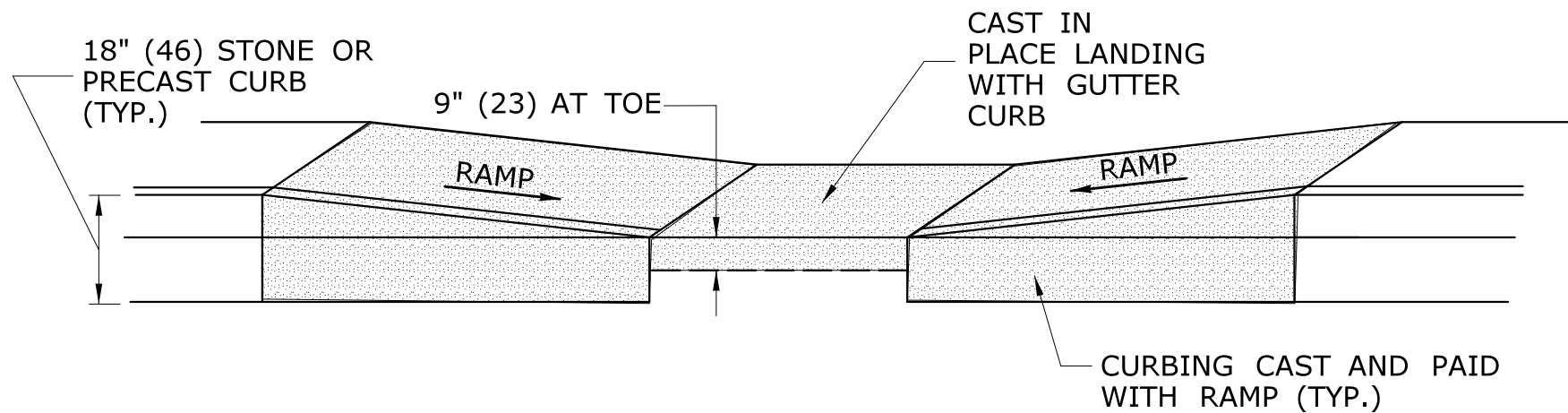
**PERPENDICULAR SIDEWALK RAMP
ONE DIRECTION ON CORNER
(TYPE 6a)**



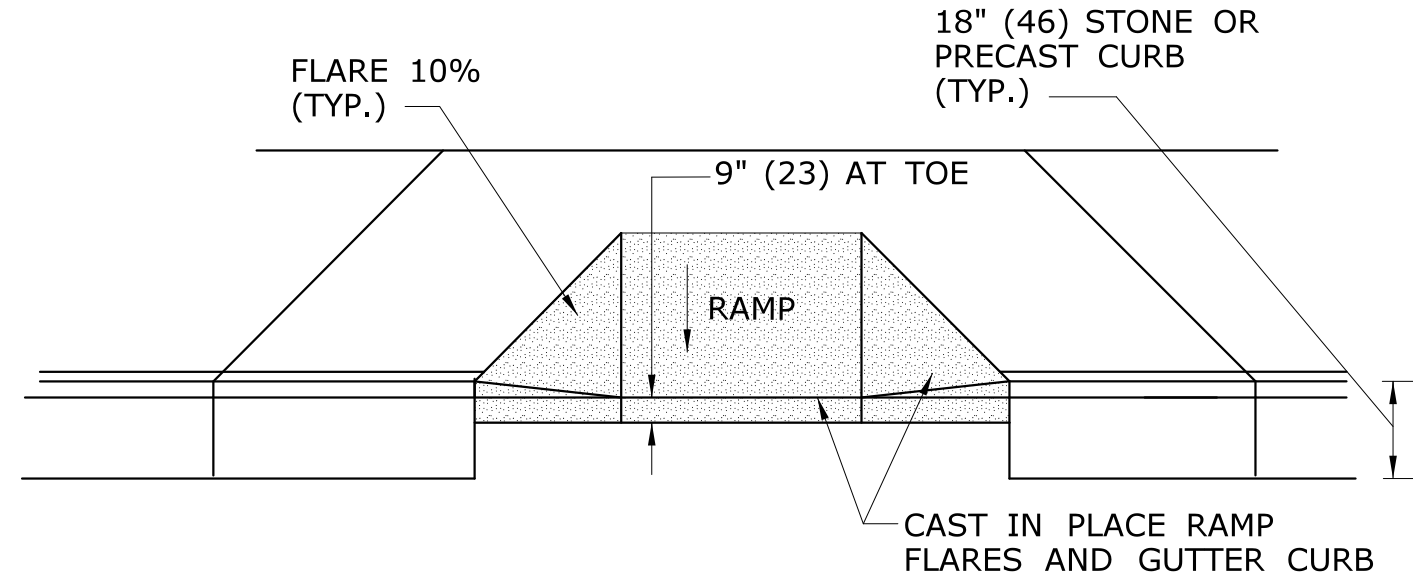
**RESTRICTED CORNER PARALLEL SIDEWALK RAMP
W/CENTER LANDING NO GRASS STRIP
(TYPE 5c)**
* SEE NOTE 20 SHEET 1



TYPICAL SECTION THRU SIDEWALK RAMP
SEE NOTE 2 AND 17 SHEET 1



**TYPICAL ELEVATION PARALLEL SIDEWALK RAMP
WITH CAST IN PLACE GUTTER**





**TYPICAL ELEVATION PERPENDICULAR SIDEWALK RAMP
WITH CAST IN PLACE GUTTER**

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

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TOWN: WESTPORT FAIRFIELD
DRAWING TITLE: SIDEWALK RAMP SHEET 3

PROJECT NO. 158-211
DRAWING NO. GDS-15
SHEET NO. 01.03.22





1. RAMPED MEDIANS SHALL HAVE A CURB RAMP AT EITHER END AND LEVEL LANDING A MINIMUM OF 5' x 5' (1.5m x 1.5m) IN BETWEEN. CUT-THROUGH MEDIANS SHALL BE A MINIMUM OF 6' (1.8m) LONG AND 5' (1.5m) WIDE. FOR ALL MEDIANS, CUT-THROUGH OR RAMPED, A 2' (610) STRIP OF DETECTABLE WARNINGS SHALL BE INSTALLED AT THE ENTRANCE AND EXIT.
2. SEE GENERAL NOTES ON SHEET 1.



NOTE: WHEN NO GATE IS PRESENT, INSTALL DETECTABLE WARNING SURFACE 12' (3.6m) FROM THE NEAREST RAIL. IF GATE IS PRESENT, INSTALL DETECTABLE WARNING 2' (610) PRIOR TO GATE. THE ROWS OF TRUNCATED DOMES IN A DETECTABLE WARNING SURFACE SHALL BE INSTALLED PARALLEL WITH THE DIRECTION OF PEDESTRIAN TRAVEL.



ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

				DESIGNER/DRAFTER: MGB/EMK		SIGNATURE/ BLOCK:		PROJECT TITLE:		TOWN:		PROJECT NO. 158-211	
				CHECKED BY: LLF		 STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION		ROUTE 15 SAFETY IMPROVEMENTS, RESURFACING ENHANCEMENTS, AND BRIDGE IMPROVEMENTS		WESTPORT FAIRFIELD		DRAWING NO. GDS-16	
THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.				SCALE AS NOTED		APPROVED BY:  DATE:				DRAWING TITLE:		SHEET NO. 01.03.23	
				Plotted Date: 7/18/2016		Filename: ...158-211-Guide_16 - Sidewalk Ramp 4.dgn				SIDEWALK RAMP SHEET 4			
REV. DATE REVISION DESCRIPTION SHEET NO.													